



## CITY OF SAINT PAUL

Christopher B. Coleman, Mayor

25 West Fourth Street  
Saint Paul, MN 55102

Telephone: 651-266-6700  
Facsimile: 651-228-3220

### TRANSPORTATION COMMITTEE OF THE PLANNING COMMISSION

Monday, February 9, 2014, 4:00 p.m. – 5:30 p.m.

*All meetings are held in the City Hall Annex 13<sup>th</sup> floor*

*Conference room at 25 West 4<sup>th</sup> Street in Saint Paul*

1. Initiation and Community Advisory Group for Highway 5 / Shepard Road Study – Michelle Beaulieu, PED, 15 minutes
2. Third Street Reconstruction – Barb Mundahl, Public Works, 15 minutes
3. Citywide Bike Plan – Reuben Collins, Public Works, 45 minutes  
\*\*\*Committee Action Requested

#### *Upcoming Transportation Committee Meetings*

- March 2<sup>nd</sup>
- March 16<sup>th</sup>

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*Meetings are open to the public. The Chair may allow five minutes for informal public comment (from non-committee members) at the beginning of each agenda as needed. Additional time may be allocated for comments or further discussion at the discretion of the Chair. Meetings will be cancelled if there is not a quorum expected, or if there are no agenda items. For additional information on the Transportation Committee of the Planning Commission, please visit our website at [bit.ly/StPaulTC](http://bit.ly/StPaulTC) or contact Hilary Holmes at [hilary.holmes@ci.stpaul.mn.us](mailto:hilary.holmes@ci.stpaul.mn.us) or 651-266-6612.*

## Transportation Committee Staff Report

*Committee date: 2/9/2015*

<b>Project Name</b>	<b>Highway 5 / Shepard Road Access Options Study</b>
Geographic Scope	Traffic modeling and engineering for the interchange between the Highway 5 bridge, Shepard Road, West 7 <sup>th</sup> Street/Fort Road, and other local roads including Edgcumbe Road, Davern Street, and Norfolk Avenue; and traffic modeling for the interchange between 35E, West 7 <sup>th</sup> Street and Shepard Road.
Ward(s)	Ward 3 (primary); Ward 2
District Council(s)	Highland Park (primary); Fort Road Federation
Project Description	This is a traffic engineering study intended to find a solution to the unsafe pedestrian conditions, including high vehicle speeds and volumes, along West 7 <sup>th</sup> and Shepard Road in the Shepard Davern Area. This study will be looking at potential redesign and reconstruction of the roadways at the intersection of Norfolk, West 7th Street, the Highway 5 bridge on and off ramps, Shepard Road, Edgcumbe, Gannon and the other roadways in the immediate area.
Project Contact	PED Michelle Beaulieu; PW Mark Finken
Contact email/phone	<a href="mailto:Michelle.Beaulieu@ci.stpaul.mn.us">Michelle.Beaulieu@ci.stpaul.mn.us</a> ; <a href="mailto:Mark.Finken@ci.stpaul.mn.us">Mark.Finken@ci.stpaul.mn.us</a>
Lead Agency/Department	Public Works (consultant and technical work); PED (community engagement and process)
Purpose of Project/Plan	To improve pedestrian conditions and access both along and across West 7th Street; a redesigned Shepard Road as a parkway-like facility; and to balance of traffic between Shepard Road and West 7th Street, creating a more connected community with better access to jobs, services, park facilities and the river.
Planning References	From Comprehensive Plan Transportation Chapter: 1.1 Complete the Streets. 1.2 Examine alternatives to enhance safety through right-of-way design, including narrowing or removing lanes on roads. 2.4 Develop a strategy for investing in a broad range of infrastructure projects, including, but not limited to, street and traffic improvements to support the growth of existing employment, services, parks, and schools. 4.11 To create livable neighborhoods and compact commercial areas, promote and fund traffic calming measures.
Project stage	Study Initiation
General Timeline	February 2015 – October 2015
District Council position (if applicable)	n/a
Level of Committee Involvement	2/9: Initiate Study; set up Community Advisory Group June – October 2015: Review study and public input; recommend preferred alignment to the Planning Commission
Previous Committee action	n/a

Level of Public Involvement	The Transportation Committee is being asked to recommend that the Planning Commission formally initiate this study, and appoint a Community Advisory Group to comment on and guide the technical consultant work.
Public Hearing	Yes, likely September 2015
Public Hearing Location	Planning Commission
Primary Funding Source(s)	City of Saint Paul
Cost	\$300,000

**PED Staff to fill in**

Staff recommendation	To recommend the study and formation of a Community Advisory Group to the Planning Commission.
Action item requested of the Committee	Make recommendation to the Planning Commission.
Committee recommendation	To be filled in at the meeting
Committee vote	To be filled in at the meeting



**CITY OF SAINT PAUL**  
*Christopher B. Coleman, Mayor*

*25 West Fourth Street  
Saint Paul, MN 55102*

*Telephone: 651-266-6565  
Facsimile: 651-228-3261*

**DATE:** February 9, 2015

**TO:** Transportation Committee

**FROM:** Michelle Beaulieu, PED and Mark Finken, PW

**SUBJECT:** Highway 5 / Shepard Road Access Options Study

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### **Introduction**

With the recent conclusion of the *Shepard Davern Area Plan* and *Zoning Study*, Saint Paul Public Works is beginning the community engagement process for the study of a potential realignment between Highway 5, West 7<sup>th</sup> Street and Shepard Road. The goal of the study is to create a more walkable and bikeable neighborhood by slowing traffic on West 7<sup>th</sup>, improving Shepard Road based on the design criteria from the *Great River Passage Plan*, and redistributing vehicular traffic between the two streets.

SRF Consulting Group has been engaged to create a detailed traffic model of the study area, draft alternative alignments, and assess their potential impact on the neighborhood and traffic flow. A project website has been set up: [www.stpaul.gov/hwy5study](http://www.stpaul.gov/hwy5study)

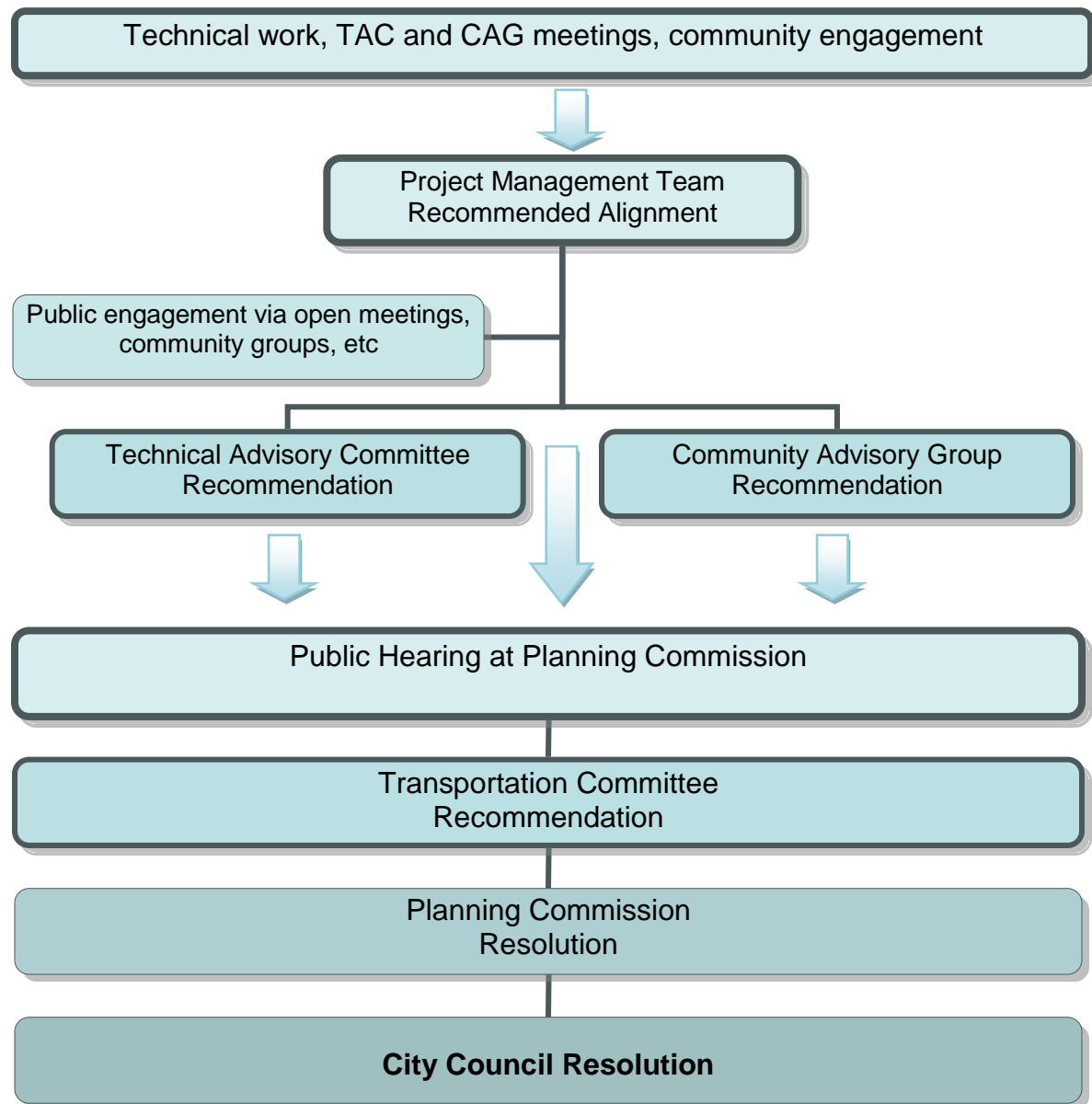
### **Project Management and the Technical Advisory Committee (TAC)**

This is a highly technical traffic and engineering study. Project Management will be led by the Public Works department, and will include other City staff and the consulting team. The Technical Advisory Committee will be comprised of technical staff from the City, MnDOT, Ramsey County and other agencies, as well as the Project Management team. Project Management will propose a preferred alignment, and TAC will make their own recommendation regarding that alignment to be considered by the Transportation Committee and Planning Commission.

### **Community Advisory Group (CAG)**

The Community Advisory Group will be comprised of residents, business representatives, major property owners, and representatives from the Mayor's office and Ward offices. The CAG will provide feedback on the technical work, and will make a recommendation on the proposed preferred alignment that will also be considered by the Transportation Committee and Planning Commission.

## Process



## Staff Recommendation

Staff recommends that the Transportation Committee approve of, and forward the attached resolution to the Planning Commission. The resolution formally initiates the planning process, as outlined above, and authorizes the formation of a Community Advisory Group.

city of saint paul  
planning commission resolution DRAFT  
file number \_\_\_\_\_  
date \_\_\_\_\_

**Highway 5 / Shepard Road Access Options Study Initiation**

**WHEREAS**, the Highway 5 bridge from Bloomington is a major entry point into Saint Paul and leads directly onto West 7<sup>th</sup> Street, which operates at high speeds with large traffic volumes; and

**WHEREAS**, Shepard Road in South Highland operates with significantly lower traffic volumes and has additional capacity for vehicular traffic; and

**WHEREAS**, both West 7<sup>th</sup> Street and Shepard Road present difficulties to pedestrians attempting to utilize the neighborhood's businesses, parks, transit stops and other amenities; and

**WHEREAS**, the *Shepard Davern Area Plan*, adopted January 21, 2015, recommends that the City slow traffic coming into the city off the Highway 5 bridge onto West 7<sup>th</sup> to improve the safety of the pedestrian environment and help create a better gateway to Saint Paul; and

**WHEREAS**, the *Great River Passage Plan*, adopted by the City Council on April 10, 2013, recommends making the Mississippi River area more accessible to the city's neighborhoods, including redesigning streets to improve park access while enhancing private development potential, and reducing traffic speeds; and

**WHEREAS**, the *Riverview Major Investment Study* from 2000 identified the need to better balance traffic between West 7<sup>th</sup> and Shepard Road, which resulted in a City Council Resolution asking MnDOT to study the intersection; and

**WHEREAS**, in 2007 Saint Paul Public Works approved a budget to study this intersection, while other partners did not secure funding, causing the study to stall; and

**WHEREAS**, Johnson Brothers is currently undergoing planning for the redevelopment of the former US Bank site on Shepard Road between Highway 5 and Davern Street, which presents an opportunity to re-think the street configuration of the area to mitigate some of the traffic issues of the area; and

**WHEREAS**, to potentially take advantage of this development timing, in 2014 SRF Consulting Group was engaged to conduct a traffic engineering study to identify potential realignments and help create a preferred alignment; and

moved by \_\_\_\_\_  
seconded by \_\_\_\_\_  
in favor \_\_\_\_\_  
against \_\_\_\_\_

**WHEREAS**, the Transportation Committee and Planning Commission will be asked to make a recommendation on a preferred alignment to the City Council; and

**WHEREAS**, that recommendation should be informed both by technical work of consultants and professional staff and by the input of community members;

**NOW, THEREFORE, BE IT RESOLVED**, that the Planning Commission hereby initiates the Highway 5 / Shepard Road Access Options Study; and

**BE IT FURTHER RESOLVED**, that the Planning Commission authorizes the creation of a Community Advisory Group to provide comment on the study process and final recommendation, to include representatives from the following groups:

- Saint Paul Planning Commission
- Saint Paul Parks Commission
- Highland District Council (2 representatives)
- Fort Road Federation
- Highland Business Association
- West 7<sup>th</sup> Business Association
- Graham Place Senior Apartments
- Highland Pointe Condominium Association
- Johnson Brothers
- Paster Properties
- Trikin Properties
- Sibley Manor
- Stewart Management
- Ward 2 Office (non-voting)
- Ward 3 Office (non-voting)
- Mayor's Office (non-voting); and

**BE IT FINALLY RESOLVED**, that this advisory group shall be appointed after review by the chair of the Planning Commission, and shall be chaired by a Planning Commissioner.

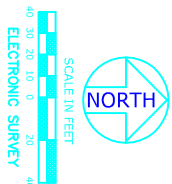
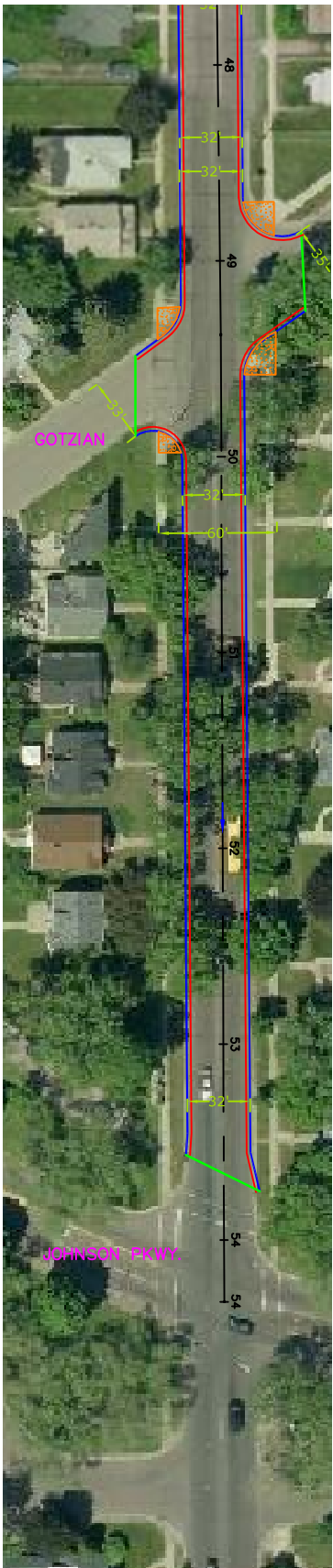
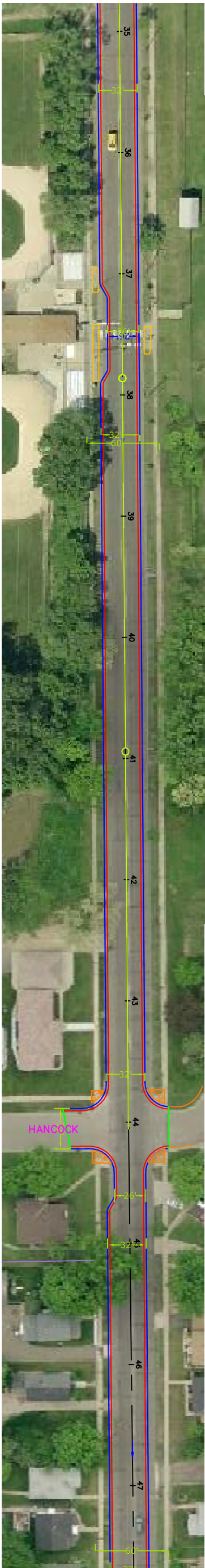
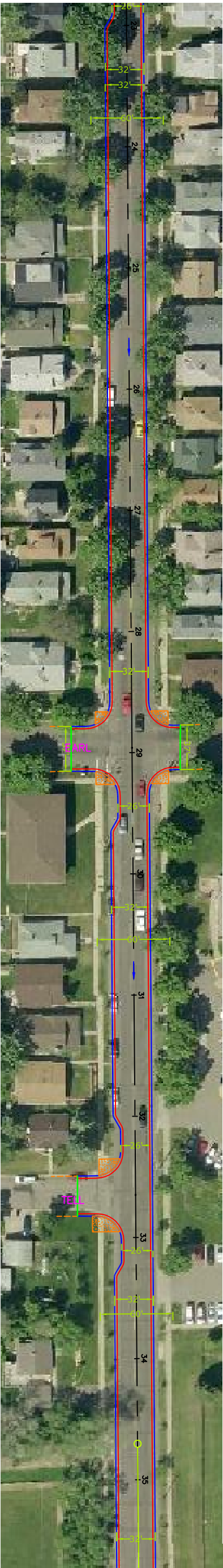
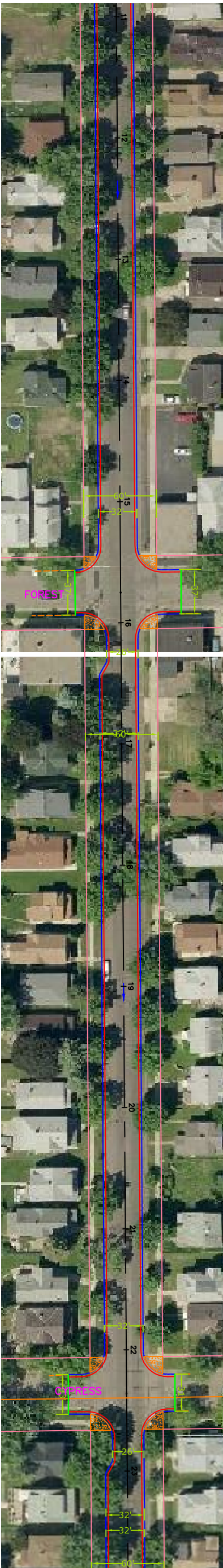
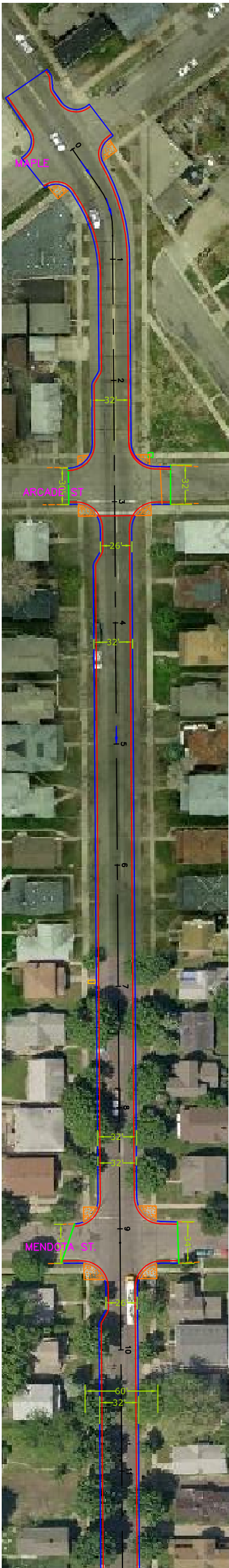
## Transportation Committee Staff Report

Committee date: February 9, 2015

<b>Project Name</b>	<b><i>THIRD STREET Paving Project</i></b>
Geographic Scope	<i>Third Street between Arcade Street and Johnson Parkway</i>
Ward(s)	<i>Ward 7</i>
District Council(s)	<i>District 4, Dayton's Bluff</i>
Project Description	<i>New concrete curb &amp; gutter, new pavement, sidewalks, designated bike lanes, lantern style street lighting and traffic signals</i>
Project Contact	<i>Barb Mundahl</i>
Contact email/phone	<i>Barbara.mundahl@ci.stpaul.mn.us</i>
Lead Agency/Department	<i>Public Works</i>
Purpose of Project/Plan	<i>To improve safety and functionality of the roadway</i>
Planning References	
Project stage	<i>Engineering</i>
General Timeline	<i>Advertise for Bid by March 31, 2015, build June through November 2015</i>
District Council position (if applicable)	<i>Supports</i>
Level of Committee Involvement	<i>Inform</i>
Previous Committee action	<i>none</i>
Level of Public Involvement	<i>Inform, advise &amp; consent, involve, development of project</i>
Public Hearing	<i>To be determined</i>
Public Hearing Location	<i>City Council</i>
Primary Funding Source(s)	<i>Street Improvement Bonds and Assessments</i>
Cost	<i>\$3,500,000</i>

Staff recommendation	<i>N/A</i>
Action item requested of the Committee	<i>N/A</i>
Committee recommendation	
Committee vote	





THIRD STREET  
Arcade to Johnson Pkwy.

CITY OF ST. PAUL

Department of Public Works

CONSTRUCTION PLAN  
for  
STREET PAVING  
of

THIRD STREET

from

ARCADE STREET TO JHONSON PARKWAY  
SAINT PAUL STREET VITALITY PROGRAM

CITY PROJECT NO. 15-P-1404

includes

GRADING, CURB AND GUTTER, AGGREGATE BASE,  
BITUMINOUS PAVING, STORM SEWER, SIGNING AND STRIPING

GROSS LENGTH	5145.31	FEET	0.972	MILES
BRIDGES LENGTH	0	FEET	0.00	MILES
EXCEPTIONS LENGTH	0	FEET	0.00	MILES
NET LENGTH	5145.31	FEET	0.972	MILES

DESIGN DESIGNATION

PRESENT ADT (2014)	6,950
PROJECTED ADT (2034)	9000
FUNCTIONAL CLASSIFICATION:	ARTERIAL
NO. OF TRAFFIC LANES	2
NO. OF PARKING LANES	2
R VALUE	10
Σ N18 FACTOR	2,168,844
DESIGN LOAD (TON)	9
DESIGN SPEED (MPH)	30
STOPPING SIGHT DISTANCE BASED ON: 3.5' HEIGHT OF EYE & 2.0' HEIGHT OF OBJECT	

- PLAN SHEET INDEX
1. TITLE SHEET
  2. LEGEND AND PLAN SHEET LOCATION MAP
  3. STATEMENT OF ESTIMATED QUANTITIES
  4. SEWER CHARTS
  5. DETAILS, STANDARD PLATES
  6. TYPICAL SECTION
  7. EROSION AND SEDIMENT CONTROL PLAN
  8. RAIN GARDENS
  - 9-15. PAVING PLANS
  - ST1-ST8. SIGNING AND STRIPING
  - LT1-LT11. STREET LIGHTING PLANS (FOR INFORMATION ONLY - WORK DONE BY OTHERS)
  - S1-T12. TRAFFIC SIGNAL PLANS (FOR INFORMATION ONLY - WORK DONE BY OTHERS)

THIS PLAN CONTAINS 50 SHEETS

SPECIFICATIONS

THE 2014 EDITION OF THE MINNESOTA DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR CONSTRUCTION" SHALL GOVERN. THESE SHALL BE FURTHER AMENDED BY THE CITY OF SAINT PAUL "STANDARD SUPPLEMENTAL SPECIFICATIONS FOR CONSTRUCTION" AND THE SPECIAL PROVISIONS.

ALL TRAFFIC CONTROL DEVICES AND SIGNING SHALL CONFORM TO THE LATEST EDITION OF THE MUTCD, INCLUDING "FIELD MANUAL FOR TEMPORARY TRAFFIC CONTROL ZONE LAYOUTS"

THE SUBSURFACE UTILITY INFORMATION IN THIS PLAN IS UTILITY QUALITY LEVEL D. THIS QUALITY LEVEL WAS DETERMINED ACCORDING TO THE GUIDELINES OF C/ASCE 38.2, ENTITLED "STANDARD GUIDELINES FOR THE COLLECTION AND DEPICTION OF EXISTING SUBSURFACE UTILITY DATA".

APPROVED . . . . ., DATE . . . . .  
Manager Street Engineering/Construction Division

APPROVED . . . . ., DATE . . . . .  
Saint Paul City Engineer

RECOMMENDED . . . . ., DATE . . . . .  
FOR APPROVAL District State Aid Engineer: Reviewed for Compliance with State Aid Rules/Policy

RECOMMENDED . . . . ., DATE . . . . .  
FOR APPROVAL Approved for State Aid Funding: State Aid Engineer



5000'

DESIGNED	BRM	I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA  Signature: _____ Date: _____ BARBARA R. MUNDAHL Lic. No: _____
DRAWN	ATT	
APPROVED	BRM	

PREPARED BY STREET ENGINEERING DIVISION FOR THE CITY OF ST. PAUL, DEPARTMENT OF PUBLIC WORKS

THIRD STREET

PROJECT: 15-P-1404	STATE AID PROJECT NUMBER:
DRAWER: 12	CAD NAME: PROJECTS/CURRENT/THIRD/THIRD-TITLE
DWG. NO. 1581	DATE: 1/20/14 SHEET NO. 1 OF 15 SHEETS

## EXISTING &amp; REMOVAL LEGEND

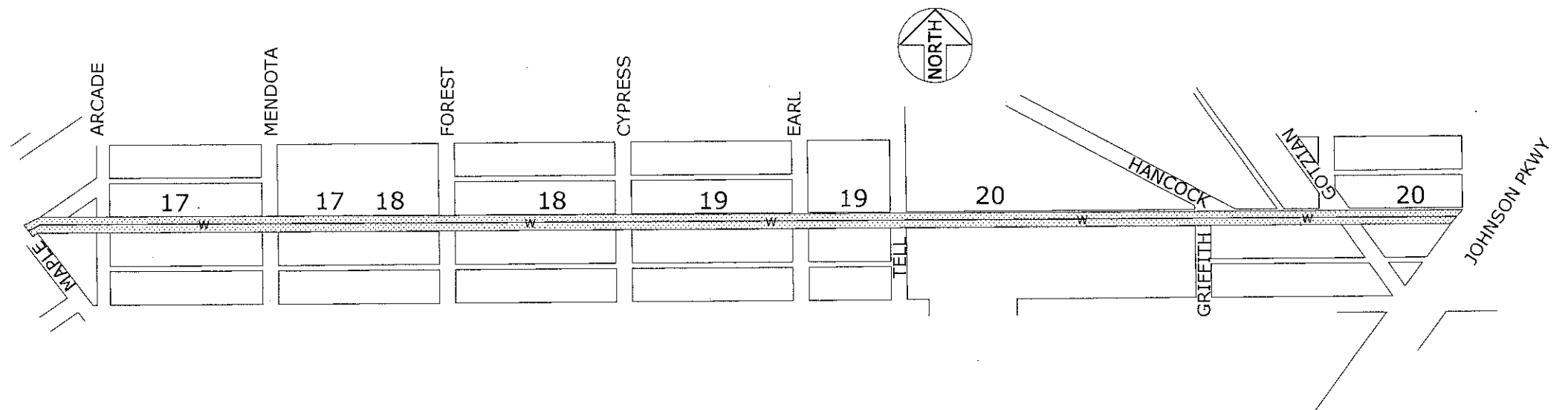
=====	EXISTING CURB & GUTTER	[A] ASPHALT DRIVE
=====	EXISTING CURB	[C] CONCRETE DRIVE
=====	EXISTING WALK	[D] DIRT DRIVE
▲	HYDRANT	[G] GRAVEL DRIVE
×	UTILITY POLE	[B] BRICK DRIVE
+	LIGHT STANDARD	
+	TRAFFIC SIGNAL	
+	PULLBOX	
○	UTILITY MANHOLE	
○	EXISTING MANHOLE (NO. REFERS TO EXISTING SEWER CHART)	
○	EXISTING CATCH BASIN (NO. REFERS TO EXISTING SEWER CHART)	
+	EXISTING SIDE INLET	
12"	EXISTING SEWER	
+	EXISTING PIPE OR C.B. LEAD, CONSTR. 8" BRICK BULKHEAD	
+	EXISTING UTILITY GATE VALVE	
+	EXISTING WATER VALVE	
W	EXISTING WATER MAIN	
G	EXISTING GAS MAIN	
T.S.	EXISTING CONDUIT (L=LIGHTING, T=TELEPHONE, E=ELECTRIC, T.S.=TRAFFIC SIGNAL, F.O.=FIBER OPTIC)	
12"	EXISTING TREE (NO. REFERS TO SIZE)	
=====	REMOVE CONCRETE CURB AND GUTTER	
=====	REMOVE CONCRETE CURB OR SANDSTONE CURB	
=====	SALVAGE GRANITE CURB	
XXXX	REMOVE CONCRETE WALK	
XXXX	REMOVE CONCRETE DRIVE	
12"	REMOVE TREE (NO. REFERS TO SIZE)	
S	REMOVE STUMP	
=====	REMOVE PAVEMENT OR TRENCH PAVEMENT	
=====	REMOVE CONCRETE STEPS	
=====	MILL BITUMINOUS SURFACE	

## CONSTRUCTION LEGEND

=====	CONSTRUCT PAVEMENT (SEE TYPICAL SECTIONS)
=====	BITUMINOUS OVERLAY
=====	PAVEMENT OR TRENCH PAVEMENT RESTORATION
=====	CONSTRUCT CURB OR CURB & GUTTER (SEE TYPICAL SECTION FOR TYPE)
=====	CONSTRUCT CURB & GUTTER (SEE TYPICAL SECTION FOR TYPE)
38	ADJUST MANHOLE (NO. REFERS TO EXISTING SEWER CHART)
○	ADJUST UTILITY MANHOLE
=====	CONSTRUCT CONCRETE WALK (4" THICK MAINLINE WALK AND OUTWALKS) (6" THICK IN PEDESTRIAN RAMP AREAS)
=====	CONSTRUCT CONCRETE DRIVEWAY PAVEMENT (TYPE 6) 6" THICK UNLESS OTHERWISE NOTED.
+	ADJUST WATER GATE VALVE HOUSING
=====	PEDESTRIAN CURB RAMP - "R" INDICATES LOCATION OF TRUNCATED DOMES
115	CONSTRUCT CATCH BASIN (NO. REFERS TO SEWER CONST. CHART)
120	CONSTRUCT CATCH BASIN LEAD (12" DIA. UNLESS OTHERWISE NOTED)
120	NEW MANHOLE- TYPE AS NOTED (NO. REFERS TO SEWER CONST. CHART)
72	NEW TYPE IV OR IV-A MANHOLE MANHOLE (NO. REFERS TO SEWER CONST. CHART)
×	CONSTRUCT POSSIBLE SANITARY SERVICE RECONNECTION
●	LEAD WATER SERVICE

## NOTES

1. WATER, GAS, ELECTRIC, TELEPHONE COMMUNICATION, SEWER AND T.V. CABLE LINES SHOWN ON THE DRAWINGS AND CROSS-SECTIONS ARE PLOTTED FROM THE BEST INFORMATION AVAILABLE AT THE TIME OF PLAN PREPARATION, BUT MAY NOT REFLECT ACTUAL LOCATIONS OR ELEVATIONS; THE CONTRACTOR SHALL VERIFY LOCATIONS OF ALL UTILITIES BEFORE BEGINNING CONSTRUCTION WHICH MAY BE AFFECTED BY A UTILITY CONFLICT. THE CONTRACTOR SHALL GIVE 48 HOURS NOTICE TO THE OWNERS OF ALL KNOWN UTILITIES BEFORE STARTING ANY OPERATIONS AFFECTING THOSE PROPERTIES, OR BEGINNING EXCAVATION IN THE VICINITY OF THOSE PROPERTIES. THE CONTRACTOR'S ATTENTION IS DIRECTED TO SECTION 1507 IN THE STANDARD SPECIFICATIONS.
2. ALL CURB RADII 20 FEET UNLESS OTHERWISE NOTED.
3. CONSTRUCT A 3 FOOT CONCRETE OUTWALK AT EACH RESIDENCE OR AS DIRECTED BY THE ENGINEER.
4. 12" CATCH BASIN LEADS TO BE CLASS V  
15" CATCH BASIN LEADS TO BE CLASS V
5. WHERE EXISTING UTILITIES ARE IN CONFLICT WITH SEWER CONSTRUCTION, THE UTILITY COMPANY WILL RELOCATE, OFFSET OR SUPPORT THE UTILITY UNLESS OTHERWISE INDICATED ON THE PLAN SHEET OR STATED IN THE SPECIAL PROVISIONS.
6. WHERE EXISTING FIRE HYDRANTS AND/OR LIGHT STANDARDS CONFLICT WITH SEWER CONSTRUCTION, THEY WILL BE RELOCATED BY OTHERS.
7. GAS MAINS SHOWN ON THE PLAN SHEETS ARE EXISTING MAINS.  
IN CONJUNCTION WITH THIS SEWER AND PAVING PROJECT, XCEL MAY ABANDON SOME OF THE GAS MAINS IN THE AREA AND CONSTRUCT NEW MAINS IN THE SIDEWALK AREA.
8. SOIL CLASSIFICATION SHOWN ON SOIL BORINGS IS ACCORDING TO A.S.T.M. DESIGNATION D2487 "STANDARD TEST METHOD FOR CLASSIFICATION OF SOILS FOR ENGINEERING PURPOSES".
9. ALL CONCRETE WALK IN QUADRANT AREAS AT THE CORNERS OF INTERSECTIONS SHALL BE 6" THICK.
10. RECORDS INDICATE THERE ARE PROPERTY IRONS AT THE BLOCK CORNERS IN THE PROJECT AREA. DO NOT DISTURB THESE IRONS WHEN REMOVING CONCRETE PANELS. IF REMOVED, REPLACEMENT BY A LICENSED SURVEYOR, AT THE CONTRACTOR'S EXPENSE.



NOTE -  
LOCATIONS FOR WATER DISTRIBUTION SYSTEM BID ITEMS UNDER 2504.602, 2504.603, AND 2504.604 ARE NOT COMPLETELY SHOWN ON PLANS - WORK SHALL BE DIRECTED IN THE FIELD BY SAINT PAUL REGIONAL WATER SERVICES WITH PAYMENT MADE AT THE CONTRACT BID PRICE FOR THE APPROPRIATE BID ITEM

12

- STREETS TO BE CONSTRUCTED
- STREET PAVING & WATER MAIN REPLACEMENT
- INFILTRATION TRENCH

LEGEND  
LOCATION AREA MAP

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DESIGNED	BRB
DRAWN	ATT
APPROVED	BRM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA

Signature: *Barbara R. Mundahl* Date: 1/20/15  
Lic. No. 43099

PREPARED BY STREET ENGINEERING DIVISION FOR THE CITY OF ST. PAUL, DEPARTMENT OF PUBLIC WORKS

## THIRD STREET

PROJECT: 15-P-1404  
DRAWER: 12  
DWG. NO. 1581

STATE AID  
PROJECT NUMBER:  
CAD NAME: PROJECTS/CURRENT/THIRD/THIRD-LEGEND  
DATE: 1/20/15  
SHEET NO. 2 OF 15 SHEETS




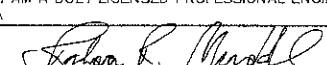
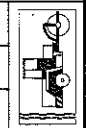


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THIRD STREET CITY PROJECT NO.						
STATEMENT OF ESTIMATED QUANTITIES						
			Participating		Non-Participating	
ITEM NO.	ITEM	UNIT	ROADWAY QUANTITY	SEWER QUANTITY	WATER QUANTITY	TOTAL QUANTITIES
2021.501	MOBILIZATION	LUMP SUM	1,000			1
2101.502	CLEARING	TREE	2			2
2101.507	GRUBBING	TREE	2			2
2101.507	TREE ROOT REMOVAL	TREE	2			2
2101.507	WOODCHIP DISPOSAL	TREE	2			2
2104.501	REMOVE CONCRETE CURB OR CURB & GUTTER	LIN. FT.	9,686			9,686
2104.503	REMOVE CONCRETE WALK	SQ. FT.				0
2104.505	REMOVE CONCRETE DRIVEWAY PAVEMENT	SQ. YD.	218			218
2104.505	REMOVE PAVEMENT	SQ. YD.	23,528			23,528
2104.505	REMOVE TRENCH PAVEMENT	SQ. YD.				0
2104.509	REMOVE CATCH BASIN OR MANHOLE	EACH	44	4		48
2104.509	REMOVE SIGN PANEL, TYPE C	EACH				0
2104.509	REMOVE SIGN, TYPE C	EACH				0
2104.513	SAWING PAVEMENT	LIN. FT.	638			638
2105.501	COMMON EXCAVATION (P)	CU. YD.	11,361			11,361
2105.607	EXCAVATION SPECIAL	CU. YD.				0
2105.607	EXCAVATION SPECIAL 1	CU. YD.				0
2105.609	GRANULAR BORROW	TON	29,142			29,142
2118.501	AGGREGATE SURFACING, CLASS 5	TON				0
2123.610	STREET SWEEPING	HOURL				0
2130.501	WATER	1000 GALLON	854			854
2211.503	AGGREGATE BASE PLACED (C.V.), CLASS 5 (P)	CU. YD.	8,245			8,245
2301.504	CONCRETE PAVEMENT 6"	SQ. YD.				0
2331.603	SAWED AND SEALED JOINT	LIN. FT.	6,426			6,426
2357.502	BITUMINOUS MATERIAL FOR TACK COAT	GALLON	1,070			1,070
2360.501	TYPE SPWEA330C WEARING COURSE MIXTURE	TON	3,417			3,417
2360.501	TYPE SPNWB330C NON-WEARING COURSE MIXTURE	TON	6,833			6,833
2360.503	TYPE SPWEA330C WEARING COURSE MIXTURE, 3" THICK	SQ. YD.	8			8
2451.505	AGGREGATE BACKFILL	CU. YD.				0
2451.609	GRANULAR BACKFILL	TON		250		250
2451.609	GRANULAR BEDDING AND/OR FOUNDATION	TON		250		250
2503.511	12" P.V.C., C-900	LIN. FT.		935		935
2503.511	8" DUCTILE IRON PIPE SEWER	LIN. FT.				0
2503.541	12" R.C. PIPE SEWER, DESIGN 3006, CLASS V	LIN. FT.				0
2503.541	15" R.C. PIPE SEWER, DESIGN 3006, CLASS V	LIN. FT.		1,045		1,045
2503.541	18" R.C. PIPE SEWER, DESIGN 3006, CLASS V	LIN. FT.				0
2503.541	12" PERFORATED THERMOPLASTIC PIPE SEWER	LIN. FT.				0
2503.602	CONNECT TO EXISTING STRUCTURE	EACH				0
2503.602	SANITARY SEWER SERVICE REPAIR EXCAVATION 14' OR LESS AT MAIN	EACH		2		2
2503.602	SANITARY SEWER SERVICE REPAIR EXCAVATION GREATER THAN 14' AT MAIN	EACH				0
2503.603	SANITARY SEWER SERVICE REPAIR 14' OR LESS AT MAIN	LIN. FT.				0
2503.603	SANITARY SEWER SERVICE REPAIR GREATER THAN 14' AT MAIN	LIN. FT.				0
2503.603	SANITARY SEWER SERVICE RECONNECTION	LIN. FT.		12		12
2503.603	TELEVIEW PROJECT SANITARY SEWER	LIN. FT.				0
2504.602	ADJUST SERVICE CURB STOP BOX	EACH				0
2504.602	REPAIR VALVE BOX	EACH				0
2504.602	ADJUST VALVE BOX	EACH				0
2504.602	SERVICE STOP BOX	EACH				0
2504.602	WATER UTILITY HOLE	EACH				0
2504.602	EXCAVATION FOR WATER MAIN OFFSET TRENCH / REPLACEMENT TRENCH	EACH				0
2504.603	WATER MAIN TRENCH	LIN. FT.				0
2504.603	WATER SERVICE EXCAVATION	LIN. FT.				0
2504.604	2" INSULATION	SO. YD.				0

THIRD STREET CITY PROJECT NO.						
STATEMENT OF ESTIMATED QUANTITIES						
			Participating		Non-Participating	
ITEM NO.	ITEM	UNIT	ROADWAY QUANTITY	SEWER QUANTITY	WATER QUANTITY	TOTAL QUANTITIES
2504.604	2" INSULATION	SQ. YD.				0
2506.503	RECONSTRUCT PRECAST CONCRETE MANHOLE	LIN. FT.				0
2506.503	RECONSTRUCT BRICK MANHOLE	LIN. FT.				0
2506.516	CASTING ASSEMBLY	EACH				0
2506.602	ADJUST MANHOLE CASTING ASSEMBLY, 0" TO 6"	EACH		51		51
2506.602	ADJUST MANHOLE CASTING ASSEMBLY, 7" TO 17"	EACH		1		1
2506.602	CONSTRUCT MANHOLE, DESIGN TYPE I	EACH		2		2
2506.602	CONSTRUCT MANHOLE, DESIGN TYPE II	EACH		1		1
2506.602	CONSTRUCT MANHOLE, DESIGN TYPE III W/2' SUMP	EACH				0
2506.602	CONSTRUCT MANHOLE SPECIAL	EACH				0
2506.602	CONSTRUCT MANHOLE, DESIGN TYPE IV	EACH				0
2506.602	CONSTRUCT CATCH BASIN, DESIGN TYPE 7A	EACH				0
2506.602	CONSTRUCT CATCH BASIN, DESIGN TYPE 7B	EACH		44		44
2506.602	FURNISH AND INSTALL CATCH BASIN OUTLET HOOD-PVC	EACH				0
2511.515	GEOTEXTILE FABRIC FILTER, TYPE II	SQ. YD.				0
2521.501	4" CONCRETE WALK	SQ. FT.	4,050			4,050
2521.501	8" CONCRETE WALK	SQ. FT.	3,762			3,762
2531.501	CONCRETE CURB & GUTTER, DESIGN B-624	LIN. FT.	10,737			10,737
2531.507	8" CONCRETE DRIVEWAY PAVEMENT	SQ. YD.	415			415
2531.507	8" CONCRETE DRIVEWAY PAVEMENT	SQ. YD.	110			110
2531.603	HAND FORM CONCRETE CURB	LIN. FT.				0
2531.618	TRUNCATED DOME	SQ. FT.	512			512
2540.602	CONSTRUCT SURVEY MONUMENT	EACH				0
2563.601	TRAFFIC CONTROL	LUMP SUM	1,000			1
2564.602	INSTALL SIGN COLLARS	EACH				0
2564.618	F&I SIGN PANEL TYPE C	SQ.FT.				0
2564.618	F&I SIGN TYPE C	SQ.FT.				0
2573.530	STORM DRAIN INLET PROTECTION	EACH		45		45
2573.602	INTERIM SEDIMENT CONTROL FOR CATCH BASINS	EACH		44		44
2574.525	TOPSOIL BORROW (L.V.)	CU. YD.				0
2575.505	SODDING, TYPE LAWN	SQ. YD.	9,518			9,518
2575.513	HARDWOOD MULCH, TYPE 6 - DOUBLE SHREDDED	CU. YD.				0
2575.560	HYDRAULIC SOIL STABILIZER, TYPE 5	POUND				0
2575.607	SELECT TOPSOIL BORROW (SOIL FOR GARDEN)	CU. YD.				0
2582.501	PAVEMENT MESSAGE (LEFT ARROW) - POLY PREFORM-INLAY	EACH				0
2582.501	PAVEMENT MESSAGE (RIGHT ARROW) - POLY PREFORM-INLAY	EACH				0
2582.501	PAVEMENT MESSAGE (SHARROW) - POLY PREFORM-INLAY	EACH				0
2582.501	PAVEMENT MESSAGE (BIKE LANE ARROW) - POLY PREFORM - INLAY	EACH				0
2582.501	PAVEMENT MESSAGE (BIKE LANE ARROW) - POLY PREFORM -GROUND IN	EACH				0
2582.502	4" SOLID LINE WHITE - POLY PREFORM (INLAY)	LIN. FT.				0
2582.502	4" BROKEN LINE YELLOW - POLY PREFORM (INLAY)	LIN. FT.				0
2582.502	4" DOUBLE SOLID LINE YELLOW - POLY PREFORM (INLAY)	LIN. FT.				0
2582.502	4" DOTTED LINE WHITE-POLY PREFORM (INLAY)	LIN. FT.				0
2582.502	8" SOLID LINE WHITE - POLY PREFORM (INLAY)	LIN. FT.				0
2582.502	12" SOLID LINE WHITE - POLY PREFORM (INLAY)	LIN. FT.				0
2582.502	4" SOLID LINE WHITE - POLY PREFORM (GROUND IN)	LIN. FT.				0
2582.502	4" DOUBLE SOLID LINE YELLOW - POLY PERFORM (GROUND IN)	LIN. FT.				0
2582.502	4" DOTTED LINE WHITE-POLY PREFORM (GROUND IN)	LIN. FT.				0
2582.502	4" BROKEN LINE YELLOW - POLY PREFORM (GROUND IN)	LIN. FT.				0
2582.502	8" SOLID LINE WHITE - POLY PREFORM (GROUND IN)	LIN. FT.				0

STATEMENT OF EST.QUANTITIES

	DESIGNED	BRM	I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA  Signature:  Date: 1/20/15 BARBARA R. MUNDAHL Lic. No. 43099	PREPARED BY STREET ENGINEERING DIVISION FOR THE CITY OF ST. PAUL, DEPARTMENT OF PUBLIC WORKS	PROJECT: 15-P-1404	STATE AID PROJECT NUMBER:	
	DRAWN	ATT				CAD NAME: PROJECTS/CURRENT/THIRD/THIRD-EST	
	APPROVED	BRM				DWG. NO. 1581	

THIRD STREET

EXISTING SEWER STRUCTURES

STR NO.	STATION	LT.	CENTER	RT.	REM CB	ADJ MH	REM MH
Third (SHEET 9)							
1	0+3.86			38.93		X	
2	0+33.78	14.67				X	
3	0+32.03			24.79		X	
4	0+84.59			18.99	X		
5	2+71.53	42.31			X		
6	2+80.44	46.69				X	
7	3+01.42	33.78			X		
8	3+28.37	0.58				X	
9	6+17.95	0.61				X	
10	8+75.12			18.22	X		
11	8+79.27	18.42			X		
12	8+96.28	40.74			X		
13	9+07.85	0.92				X	
14	9+23.25			35.76		X	
15	9+21.41	34.39				X	
16	9+27.17	43.9			X		
17	9+44.48	18.11			X		
18	9+46.78			17.83	X		
19	9+53.60			17.95	X		
95	0+36.37	17.72			X		
Third (SHEET 10)							
20	12+40.30		X			X	
21	15+56.35	38.88			X		
22	15+75.56		X			X	
23	15+81.55			34.02		X	
24	15+81.33	33.68				X	
25	15+92.83	38.9			X		
Third (SHEET 11)							
26	16+09.10	18.25			X		
27	16+08.87			17.84	X		
28	17+95.12	0.53				X	
29	17+99.89	21.54			X		
30	18+02.95			20.69	X		
31	20+15.42		X			X	
32	22+20.58			35.99	X		
33	22+30.03			39.69		X	
34	22+29.86	34.51				X	
35	22+35.91		X		X		
36	22+52.59			36.48	X		
Third (SHEET 12)							
37	22+71.23			21.77	X		
38	22+68.07	18.73			X		
39	25+52.11		X			X	
40	27+29.05	0.87				X	
41	28+51.30	19.26			X		
42	28+53.44			18.38	X		
43	28+78.17			43.88	X		
44	28+96.25		X			X	
45	29+11.47			6.61		X	
46	29+14.53			44.57	X		

PROPOSED SEWER STRUCTURES

STR NO.	STATION	LT.	CENTER	RT.	REM CB	ADJ MH	REM MH
Third (SHEET 13)							
47	29+91.76			6.31		X	
48	29+99.71			0.64		X	
49	30+82.64	1.04				X	
50	32+24.81			18.66	X		
51	32+49.05			44.26	X		
52	32+65.60			1.05		X	
53	32+71.72			5.79		X	
54	32+71.47			30.62		X	
55	32+82.35			42.86	X		
56	32+78.23	5.5				X	
57	33+71.64		X			X	
58	35+20.95			0.55		X	
59	35+23.84	5.48				X	
60	35+69.78	17.99			X		
61	35+80.46			10.13		X	
62	35+71.41			18.71	X		
63	36+33.47			10.26		X	
64	37+80.27			19.3	X		
Third (SHEET 14)							
65	38+05.26			19.28	X		
66	38+09.78			9.75		X	
67	38+07.99	17.82			X		
68	38+15.29		X		X		
69	39+62.94		X			X	
70	41+02.24	18.28			X		
71	41+03.27			19.27	X		
72	41+09.14		X			X	
73	42+20.80			9.14		X	
74	43+88.64			42.75	X		
75	43+94.81	24.37				X	
76	44+05.70	15.44				X	
77	44+05.80		X			X	
78	44+13.95	23.18				X	
79	44+14.77			6.25		X	
80	44+15.19			44.33		X	
81	44+21.74			44.72	X		
82	44+23.91			0.57		X	
83	44+37.78	18.48			X		
84	44+43.05			19.19	X		
85	45+38.38	17.53			X		
86	46+45.33			1.46		X	
87	46+50.56			7.58		X	
88	46+55.71			20.19	X		
Third (SHEET 15)							
89	48+66.75			8.25		X	
90	48+64.12	30.73				X	
91	48+80.32			2.37		X	
92	49+33.33	32.9			X		
93	49+39.69			2.72		X	
94	51+74.84			0.94		X	

STR NO.	STATION	LT.	CENTER	RT.	MH	CB	DATE	15" RCP	18" RCP	12" PVC C-900
Third (SHEET 9)										
200	0+34.90	14.05				7B				
201	0+85.49			18.75		7B		39.7		
202	2+71.75	39.28				7B		11.4		
203	3+00.59	39.15				7B		26.8		
204	8+51.23	12.08				7B		16		
205	8+67.23	12.09				7B		37.7		
206	8+68.78			15.95		7B		57.9		
207	8+96.29	40.35				7B			22.9	
208	9+26.78	41.16				7B		5.6		
209	9+55.71	11.95				7B			40.9	
210	9+71.71	12				7B		16		
211	9+80.72			16.02		7B			60.8	
Third (SHEET 10)										
212	9+96.72			16.13		7B		14		
213	15+58.47	39.07				7B		23.5		
214	15+91.24	38.77				7B		11.1		
Third (SHEET 11)										
215	16+18.18	11.96				7B			58.8	
216	16+45.89			16.02		7B		36.6		
217	22+22.23			43.62		7B		8.7		
218	22+50.78			37.67		7B		20.9		
Third (SHEET 12)										
219	22+75.98	11.96				7B			51.2	
220	23+05.94			15.95		7B		38.2		
221	28+50.62	12.04				7B		26		
222	28+51.06			15.96		7B		61.2		
223	28+79.99			45.07		7B		31.1		
224	29+13.08			45.33		7B		38.8		
Third (SHEET 13)										
225	31+96.49			15.99		7B		2.5		
226	32+04.08			5.91	TYPE I					
227	32+17.43	11.81				7B		18.5		
228	32+50.78			38.82		7B		22.3		
229	32+80.16			38.9		7B		12		
230	34+70.27		X		TYPE I					316
231	35+76.29			16.07		7B		7.1		
232	35+76.55	11.98				7B		22.5		
233	37+86.26		X		TYPE I					308
234	37+93.97	11.94				7B		14		
235	37+92.96			16.07		7B		14		
Third (SHEET 14)										
236	38+08.96			16.16		7B			6.5	
237	38+09.97	11.94				7B			21.7	
238	40+94.27		X		TYPE I					311.6
239	41+03.28	13.96				7B		30		
240	41+03.02			18.09		7B			9.5	
241	43+90.46			43.13		7B		25		
242	44+19.62			37.24		7B		8.3		
243	44+47.01	11.67				7B			35	
244	44+75.35			16.42		7B		37.1		
245	45+25.16	13.57				7B		14.1		
246	46+50.42			19.11		7B		11.5		
Third (SHEET 15)										
247	49+38.84	24.57				7B		27.3		

SEWER STRUCTURES CHART

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DESIGNED BRM  
DRAWN ATT  
APPROVED BRM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA  
Signature: *Barbara R. Munda* Date: 1/20/15  
BARBARA R. MUNDAHL Lic. No. 43099

PREPARED BY STREET ENGINEERING DIVISION FOR THE CITY OF ST. PAUL, DEPARTMENT OF PUBLIC WORKS

THIRD STREET

PROJECT: 15-P-1404

DRAWER: 12

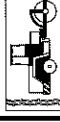
DWG. NO. 1581

STATE AID PROJECT NUMBER:

CAD NAME: PROJECTS/CURRENT/THIRD STREET/SEWER CHARTS

DATE: 1/20/15

SHEET NO. 4 OF 15 SHEETS

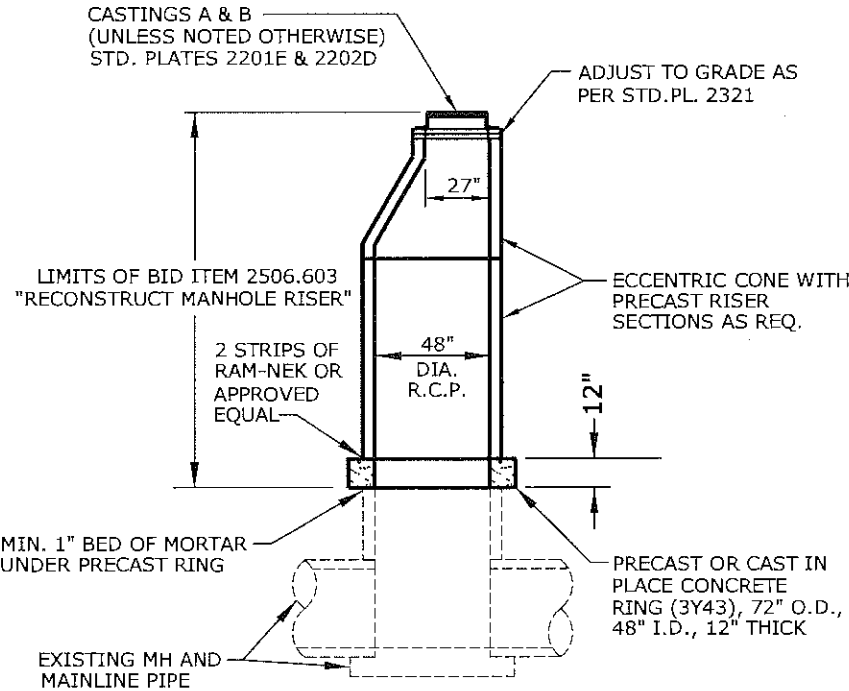


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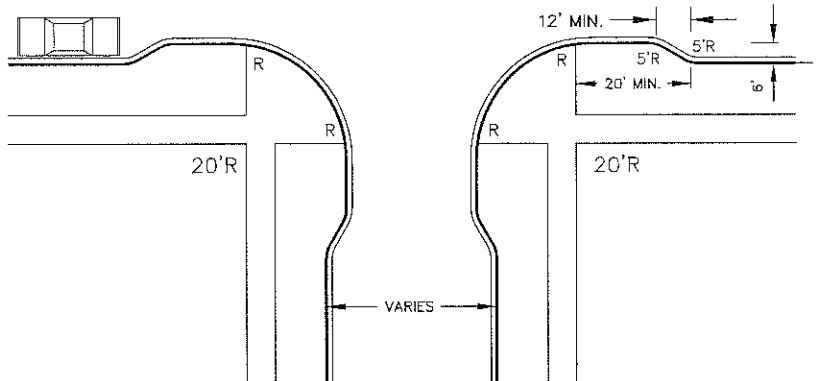
STANDARD DETAIL PLATES	
ST. PAUL DRG. NO.	STANDARD DETAIL
1206C	CONCRETE DRIVEWAY, TYPE 6
1304C	SIGN COLLAR PLACEMENT
2007E	CATCH BASIN, DESIGN TYPE 3C
2015G	CATCH BASIN, DESIGN TYPE 7A
2016	CATCH BASIN, DESIGN TYPE 7B
2101D	MANHOLE, TYPE I
2102D	MANHOLE, TYPE II
2103E	MANHOLE, TYPE III & III-S
2104E	MANHOLE, TYPE IV
2107C	MANHOLE, TYPE VII
2201E	FRAME CASTING A
2202D	COVER CASTINGS B
2203	COVER CASTINGS D
2207D	MANHOLE ADAPTOR RINGS
2211	CATCH BASIN, DESIGN TYPE 7A CASTING DETAILS
2212	MODIFIED TYPE 7A
2306E	RECONNECTION OF HOUSE SERVICES
2309C	CATCH BASIN CONNECTION
2311B	ANCHOR CLAMP DETAIL
2317C	TYPES OF TRENCH BEDDING
2318C	CONCRETE SADDLE FOR PIPE CROSSINGS
2319D	SADDLE CONNECTIONS FOR HOUSE SERVICES
2321	MANHOLE ADJUST
2322A	MANHOLE RECONSTRUCT
3000B	CONCRETE CURB
3003B	PEDESTRIAN CURB RAMP FOR THE HANDICAPPED
3005D	PEDESTRIAN CURB RAMP FOR THE HANDICAPPED
3006D	PEDESTRIAN CURB RAMP FOR THE HANDICAPPED
3008A	SAWED / SEALED JOINTS
3100C	CONCRETE CURB AND GUTTER, DESIGN B
3102B	CONC. CURB & GUTTER OPENING FOR DRIVEWAYS
3107A	CONC. CURB & GUTTER DESIGN D-418
4031B	BARRICADE DESIGNS
4032A	BARRICADING FOR STREET CLOSURE
4035A	WARNING LIGHTS AND CHANNELIZING DEVICES
4036	BARRICADING FOR SIDEWALK REPLACEMENT
6200B	RESET MONUMENT
MnDOT STANDARD DETAIL PLATES	
THESE STANDARD PLATES AS APPROVED BY THE FHWA SHALL APPLY	
MnDOT DRG. NO.	STANDARD DETAIL
3000L	REINFORCED CONCRETE PIPE
3006G	GASKET JOINT FOR R. C. PIPE
3100G	CONCRETE APRON FOR REINFORCED CONCRETE PIPE
3133D	RIPRAP AT R.C.P. OUTLETS
3145G	CONCRETE PIPE TIES
4010H	CONCRETE SHORT CONE & ADJUSTING RING
7038A	DETECTABLE WARNING SURFACE TRUNCATED DOMES
8000I	STANDARD BARRICADES

NOTES:

1. LIMITS OF MANHOLE RECONSTRUCTION TO BE DESIGNATED BY THE ENGINEER.
2. REMOVAL OF THE EXISTING CASTING AND RISER TO BE INCLUDED AS PART OF BID ITEM 2506.603, "RECONSTRUCT MANHOLE RISER".
3. PRECAST RISER SECTION TO MEET ASTM C-478
4. JOINTS BETWEEN PRECAST SECTIONS SHALL USE O-RING RUBBER GASKET CONFORMING TO ASTM C-443



BID ITEM 2506.603,  
"RECONSTRUCT MANHOLE RISER"



CURB BUMPOUT DETAILS  
NO SCALE

CONSTRUCTION DETAILS  
STANDARD DETAIL PLATES



DESIGNED	BRM
DRAWN	ATT
APPROVED	BRM

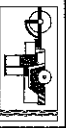
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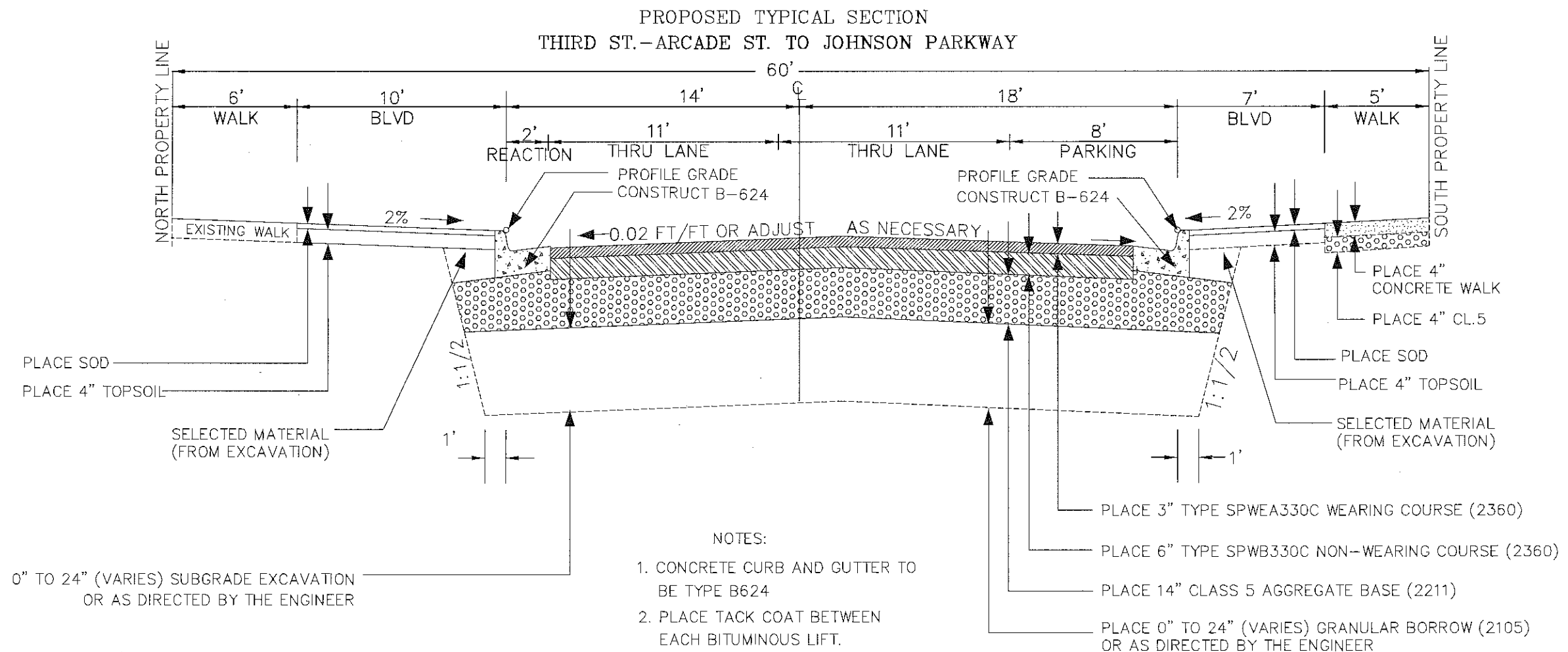
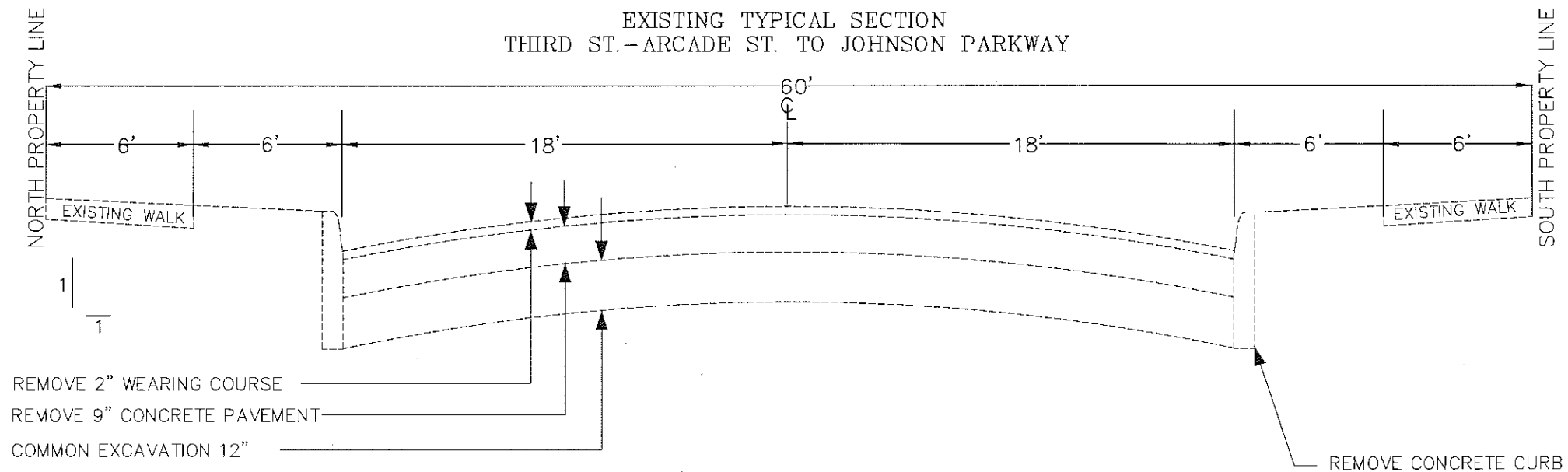
Signature: *Barbara R. Munda* Date: 1/20/15  
BARBARA R. MUNDAHL Lic. No. 43099

PREPARED BY STREET ENGINEERING DIVISION FOR THE CITY OF ST. PAUL, DEPARTMENT OF PUBLIC WORKS

THIRD STREET

PROJECT:	15-P-1404	STATE AID PROJECT NUMBER:	
DRAWER:	12	CAD NAME:	PROJECTS/CURRENT/THIRD/
DWG. NO.	1581	DATE:	1/20/15
		SHEET NO.	5 OF 15 SHEETS





THIRD STREET  
ARCADE TO JOHNSON PKWY

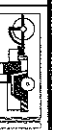
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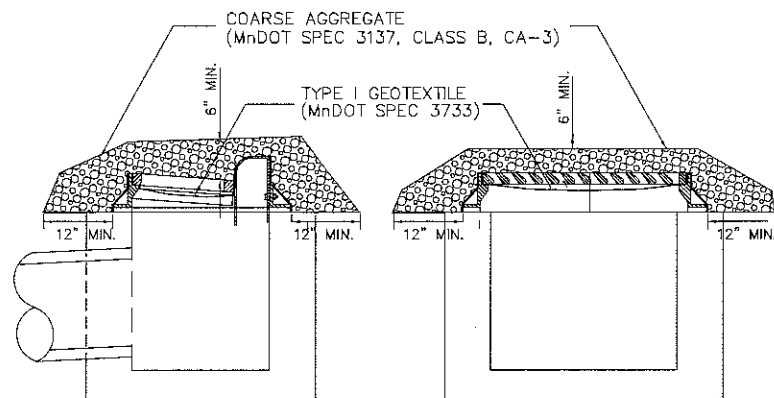
DESIGNED	BRM	I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA	
DRAWN	ATT	Signature: <i>Barbara R. Mundaahl</i>	Date: 1/20/15
APPROVED	BRM	BARBARA R. MUNDAHL Lic. No. 43099	

PREPARED BY STREET ENGINEERING DIVISION FOR THE CITY OF ST. PAUL, DEPARTMENT OF PUBLIC WORKS

THIRD STREET

PROJECT: 15-P-1404	STATE AID PROJECT NUMBER:
DRAWER: 12	CAD NAME: PROJECTS/CURRENT/THIRD/Third-typicals
DWG. NO. 1581	DATE: 1/20/15
SHEET NO. 6 OF 15 SHEETS	

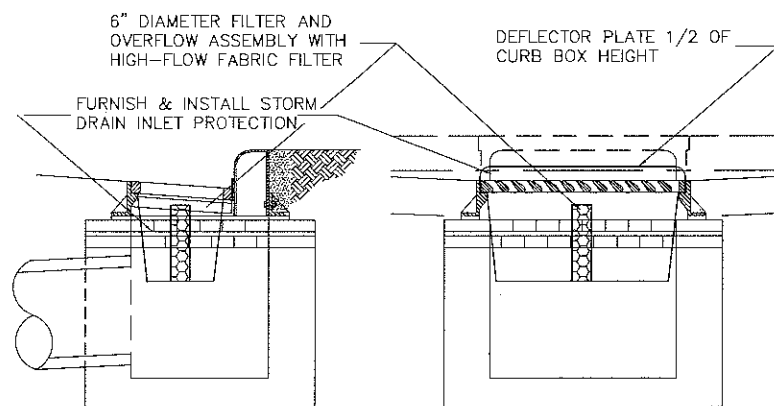
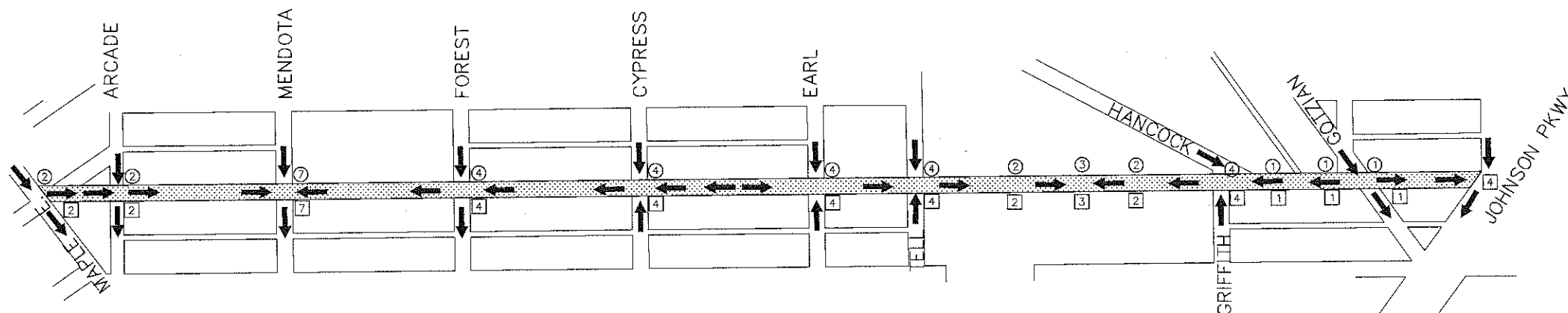




### INTERIM SEDIMENT CONTROL FOR CATCH BASINS

#### NOTES:

1. INTERIM SEDIMENT CONTROL REQUIRED FOR THE IDENTIFIED NEW CATCH BASINS PRIOR TO CURB AND GUTTER CONSTRUCTION. BID ITEM 2573.602.
2. CONTRACTOR TO INSPECT "INTERIM SEDIMENT CONTROL FOR CATCH BASINS" EVERY 7 DAYS AND WITHIN 24 HOURS AFTER EVERY RAIN EVENT. CONTRACTOR SHALL REPLACE COARSE AGGREGATE AND/OR GEOTEXTILE IF IT BECOMES NONFUNCTIONAL AS A SEDIMENT CONTROL.
3. CATCH BASINS SHALL BE KEPT FUNCTIONAL. CONTRACTOR SHALL CLEAN IF SEDIMENT IS WITHIN 12 INCHES OF OUTLET PIPE.



### STORM DRAIN INLET PROTECTION

#### NOTES:

1. STORM DRAIN INLET PROTECTION REQUIRED FOR NEW CATCH BASINS AFTER CURB AND GUTTER CONSTRUCTION. BID ITEM 2573.530.
2. CONTRACTOR TO INSPECT "STORM DRAIN INLET PROTECTION" EVERY 7 DAYS AND WITHIN 24 HOURS AFTER EVERY RAIN EVENT. CONTRACTOR SHALL REPLACE OR CLEAN INSERT IF IT BECOMES NONFUNCTIONAL.
3. CATCH BASINS SHALL BE KEPT FUNCTIONAL. CONTRACTOR SHALL CLEAN IF SEDIMENT IS WITHIN 12 INCHES OF OUTLET PIPE.
4. EXISTING CATCH BASINS AS IDENTIFIED SHALL ALSO RECEIVE STORM DRAIN INLET PROTECTION FROM THE TIME UPSTREAM CONSTRUCTION ACTIVITIES BEGIN UNTIL CLASS V IS PLACED ON THE ROADWAY AND THE BOULEVARDS HAVE BEEN RESTORED.



#### LEGEND:

- ➔ DIRECTION OF FLOW
- ② INTERIM SEDIMENT CONTROL AND STORM DRAIN INLET PROTECTION FOR NEW CATCH BASINS  
SEE DETAIL (NO. INDICATES CB'S REQUIRING CONTROL)
- ② EXISTING CATCH BASINS REQUIRING STORM DRAIN INLET PROTECTION  
SEE DETAIL (NO. INDICATES CB'S REQUIRING CONTROL)

////// LOCATION OF INFILTRATION BMP

### EROSION AND SEDIMENT CONTROL



DESIGNED	BRM
DRAWN	ATT
APPROVED	BRM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA

Signature: *Barbara R. Mundahl*

BARBARA R. MUNDAHL

Date: 1/20/15  
Lic. No. 43099

PREPARED BY STREET ENGINEERING DIVISION FOR THE CITY OF ST. PAUL, DEPARTMENT OF PUBLIC WORKS

## THIRD STREET

PROJECT: 15-P-1404

DRAWER: 12

DWG. NO. 1581

STATE AID  
PROJECT NUMBER:

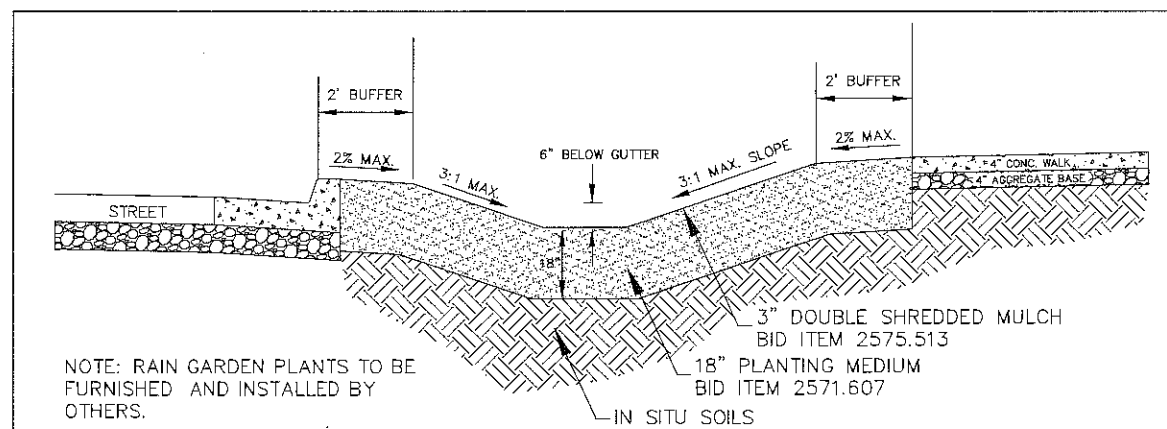
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DATE: 1/20/15

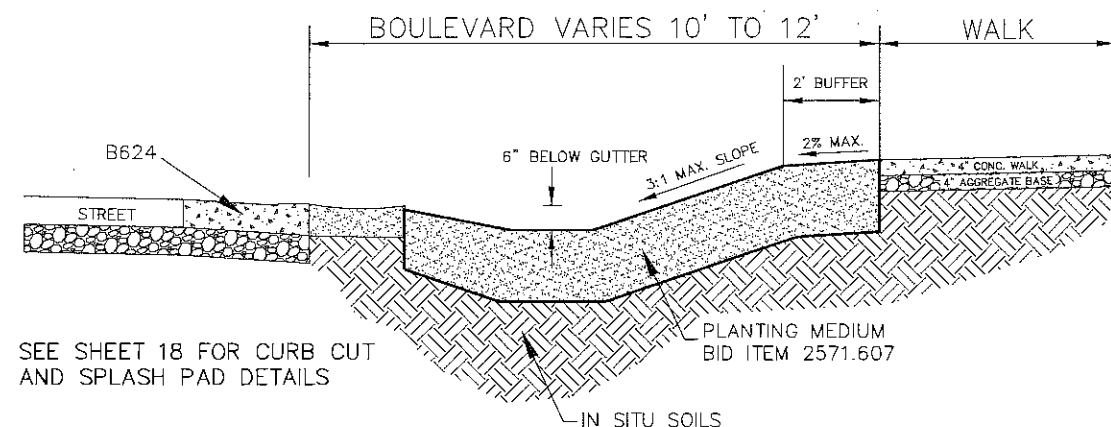
SHEET NO. 7 OF 15 SHEETS







RAIN GARDEN TYPICAL SECTION

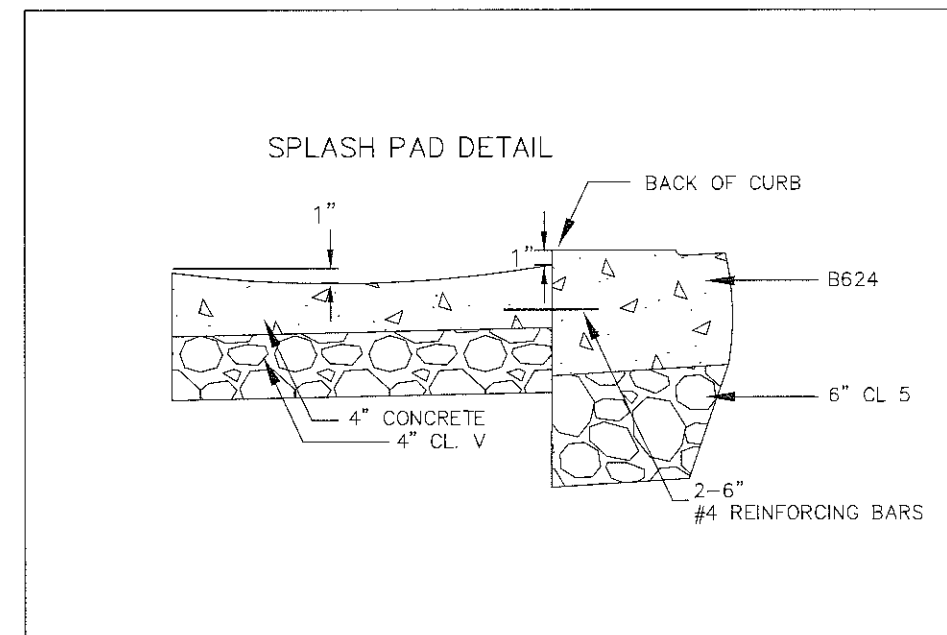


CURB CUT WITH SPLASH PAD

NOTES:

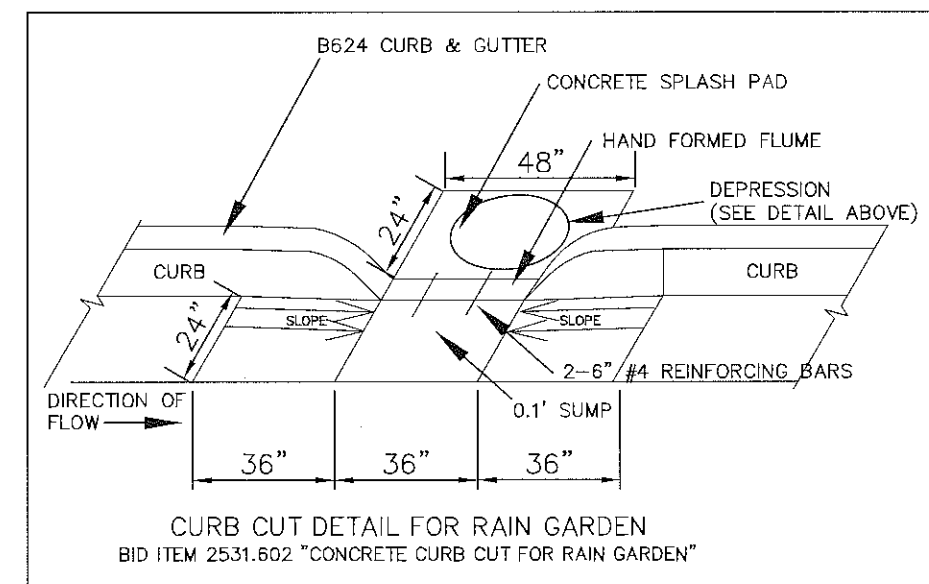
1. RAIN GARDEN DIMENSIONS AND LOCATIONS TO BE DETERMINED BY THE ENGINEER.
2. EXCAVATE RAIN GARDEN TO 6" BELOW GUTTER. BEFORE BACKFILLING WITH PLANTING MEDIUM, THE EXISTING SUB-SOILS MUST BE THOROUGHLY SCARIFIED TO A DEPTH OF 6".
3. THE PLANTING MEDIUM AND MULCH SHALL BE PLACED IN SUCH A WAY AS TO LIMIT THE AMOUNT OF COMPACTION OF THE SUB-SOILS.

BOULEVARD RAIN GARDEN DETAILS



NOTE:

CONTRACTOR TO CREATE 1" DEPRESSION IN BOTH DIRECTIONS IN SURFACE OF SPLASH PAD



RAIN GARDEN DETAILS

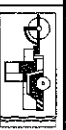
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DESIGNED	BRM	I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA Signature: <i>Barbara R. Munda</i> Date: 1/20/15 BARBARA R. MUNDAHL Lic. No. 43099
DRAWN	ATT	
APPROVED	BRM	

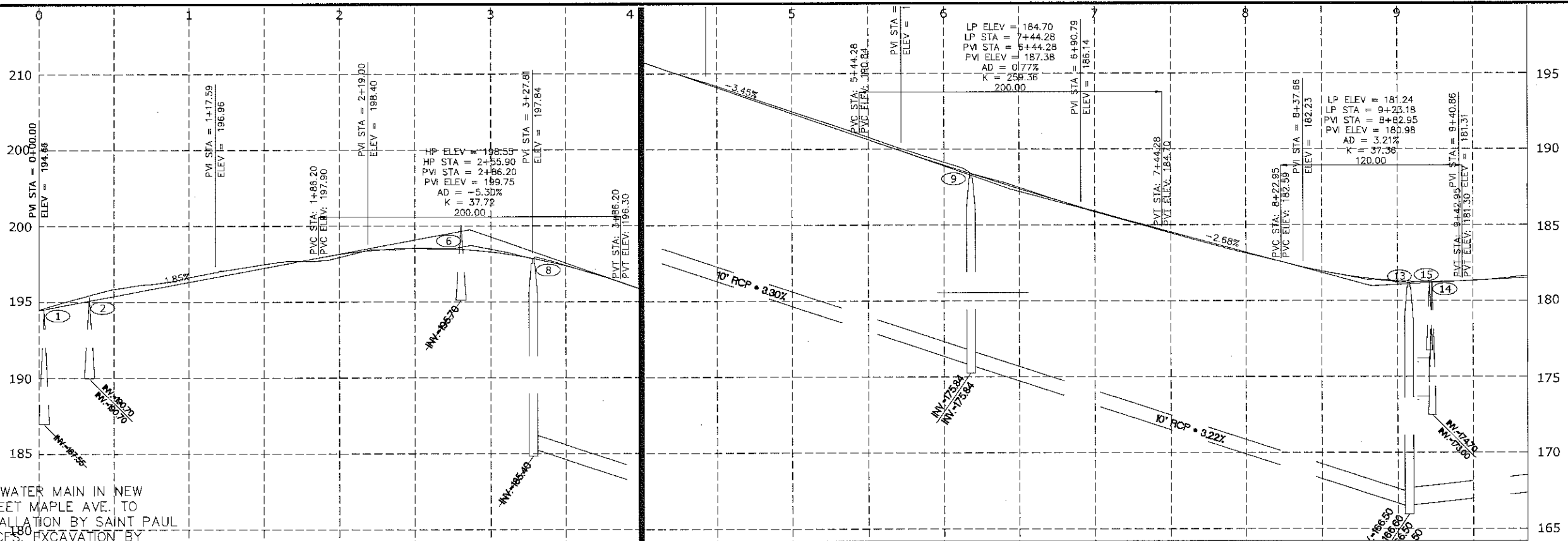
PREPARED BY STREET ENGINEERING DIVISION FOR THE CITY OF ST. PAUL, DEPARTMENT OF PUBLIC WORKS

THIRD STREET

PROJECT: 15-P-1404	STATE AID PROJECT NUMBER:
DRAWER: 12	CAD NAME: PROJECTS/CURRENT/MONTREAL/THIRD-RAINGARDEN
DWG. NO. 1581	DATE: 1/20/15 SHEET NO. 8 OF 15 SHEETS



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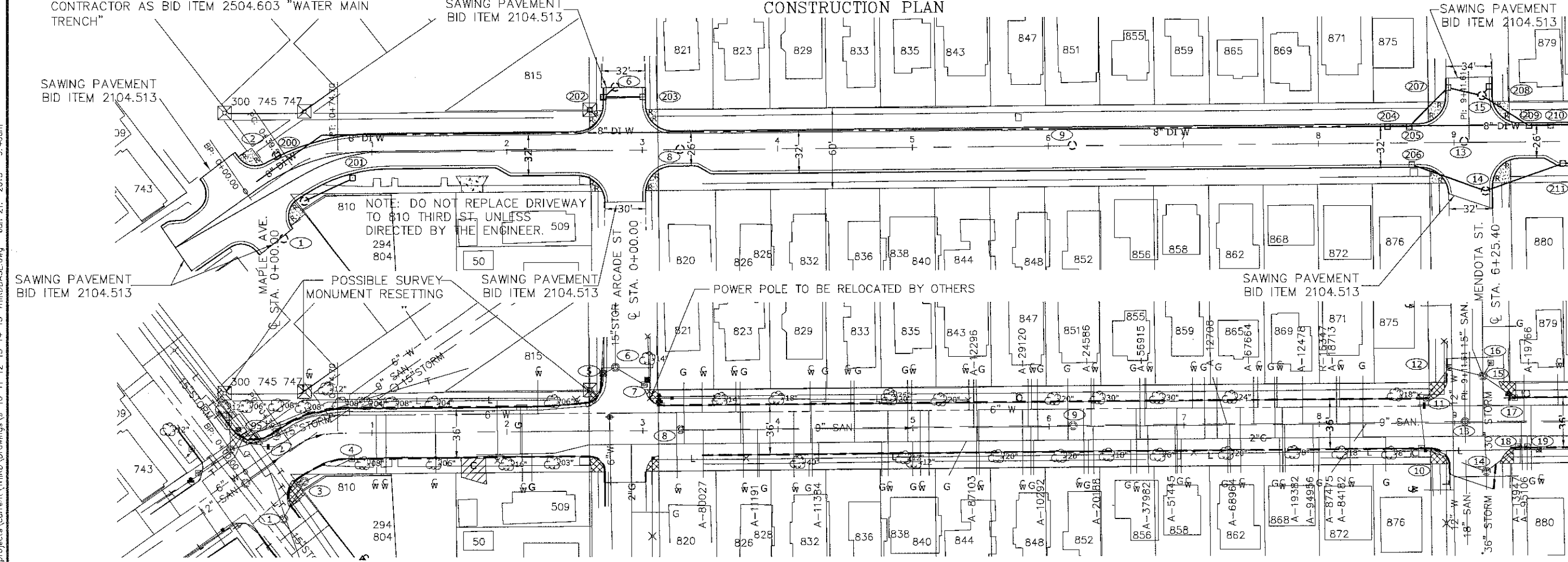


APPROX. xxxx' OF 8" DI WATER MAIN IN NEW LOCATION IN THIRD STREET MAPLE AVE. TO MENDOTA ST. PIPE INSTALLATION BY SAINT PAUL REGIONAL WATER SERVICES, EXCAVATION BY CONTRACTOR AS BID ITEM 2504.603 "WATER MAIN TRENCH"

SAWING PAVEMENT  
BID ITEM 2104.513

### CONSTRUCTION PLAN

SAWING PAVEMENT  
BID ITEM 2104.513



SAWING PAVEMENT  
BID ITEM 2104.513

NOTE: DO NOT REPLACE DRIVEWAY TO 810 THIRD ST. UNLESS DIRECTED BY THE ENGINEER. 509

SAWING PAVEMENT  
BID ITEM 2104.513

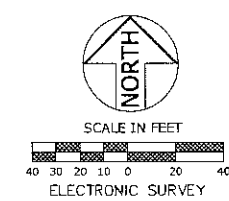
POWER POLE TO BE RELOCATED BY OTHERS

SAWING PAVEMENT  
BID ITEM 2104.513

### EXISTING AND REMOVAL PLAN

THIRD STREET  
Maple Ave. to Mendota st.

THIRD STREET

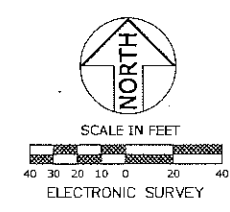
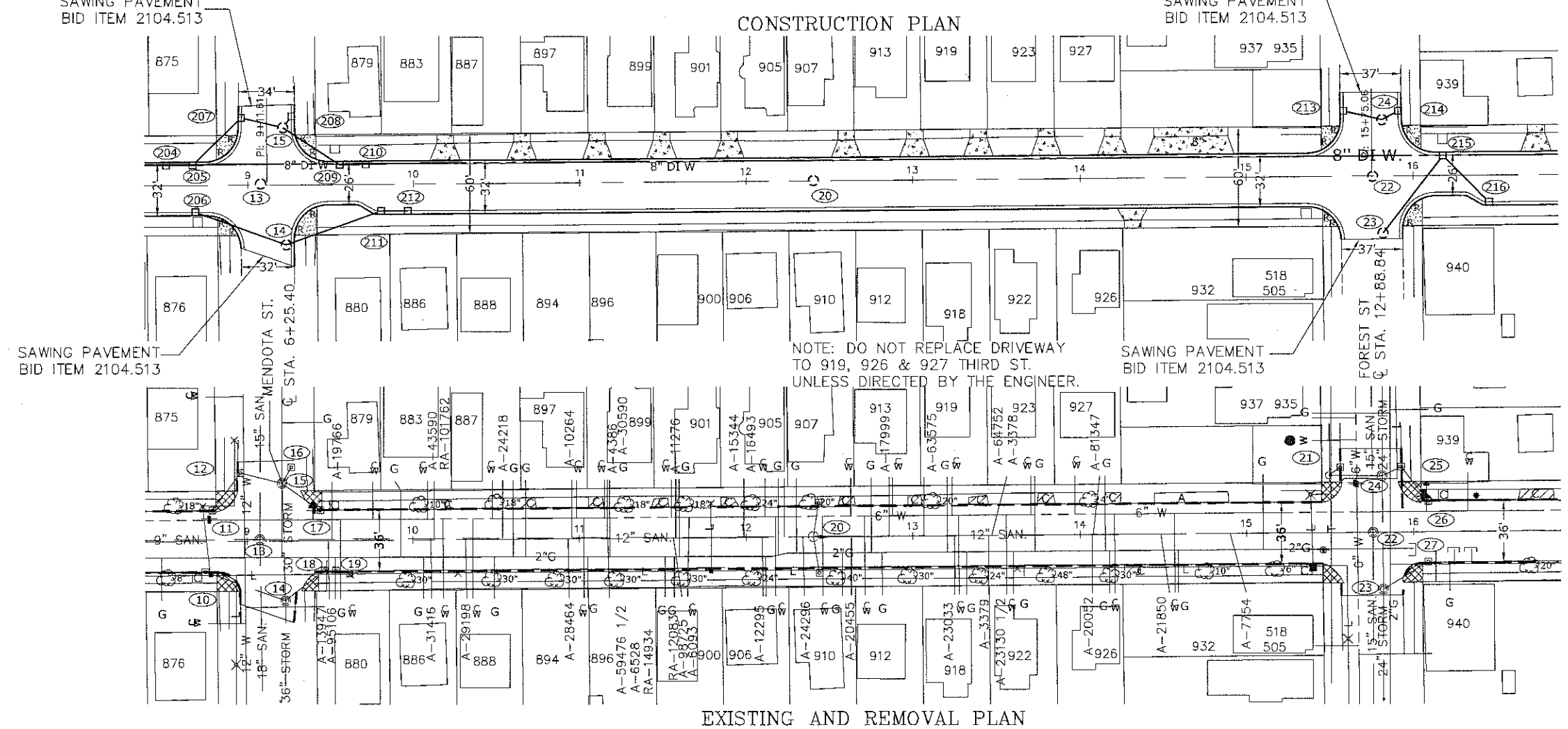
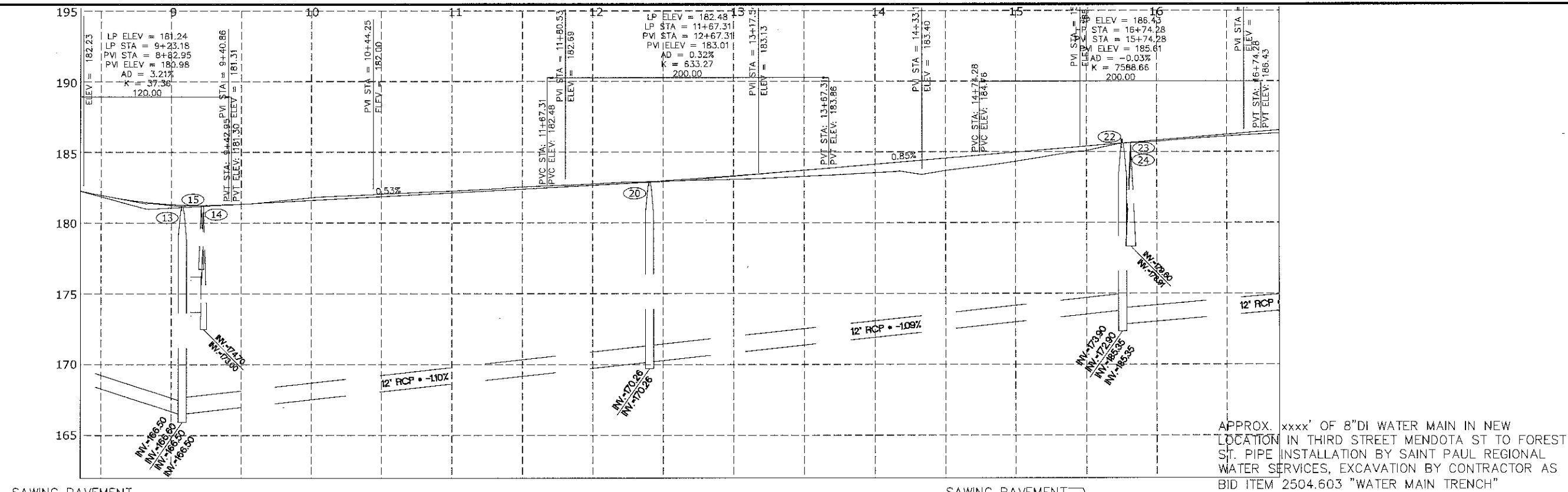


THIRD STREET

	DESIGNED	BRM	I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA  Signature: _____ Date: 1/21/15 Lic. No. 43099	PREPARED BY	STREET ENGINEERING DIVISION FOR THE CITY OF ST. PAUL, DEPARTMENT OF PUBLIC WORKS	PROJECT:	15-P-1404	STATE AID PROJECT NUMBER:	
	DRAWN	ATT		DRAWER:	12	CAD NAME:	PROJECTS/CURRENT/		
	APPROVED	BRM		DWG. NO.	1581	DATE:	1/21/15	SHEET NO.	9 OF 15 SHEETS

## THIRD STREET

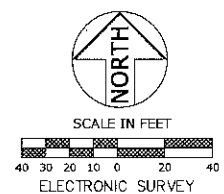
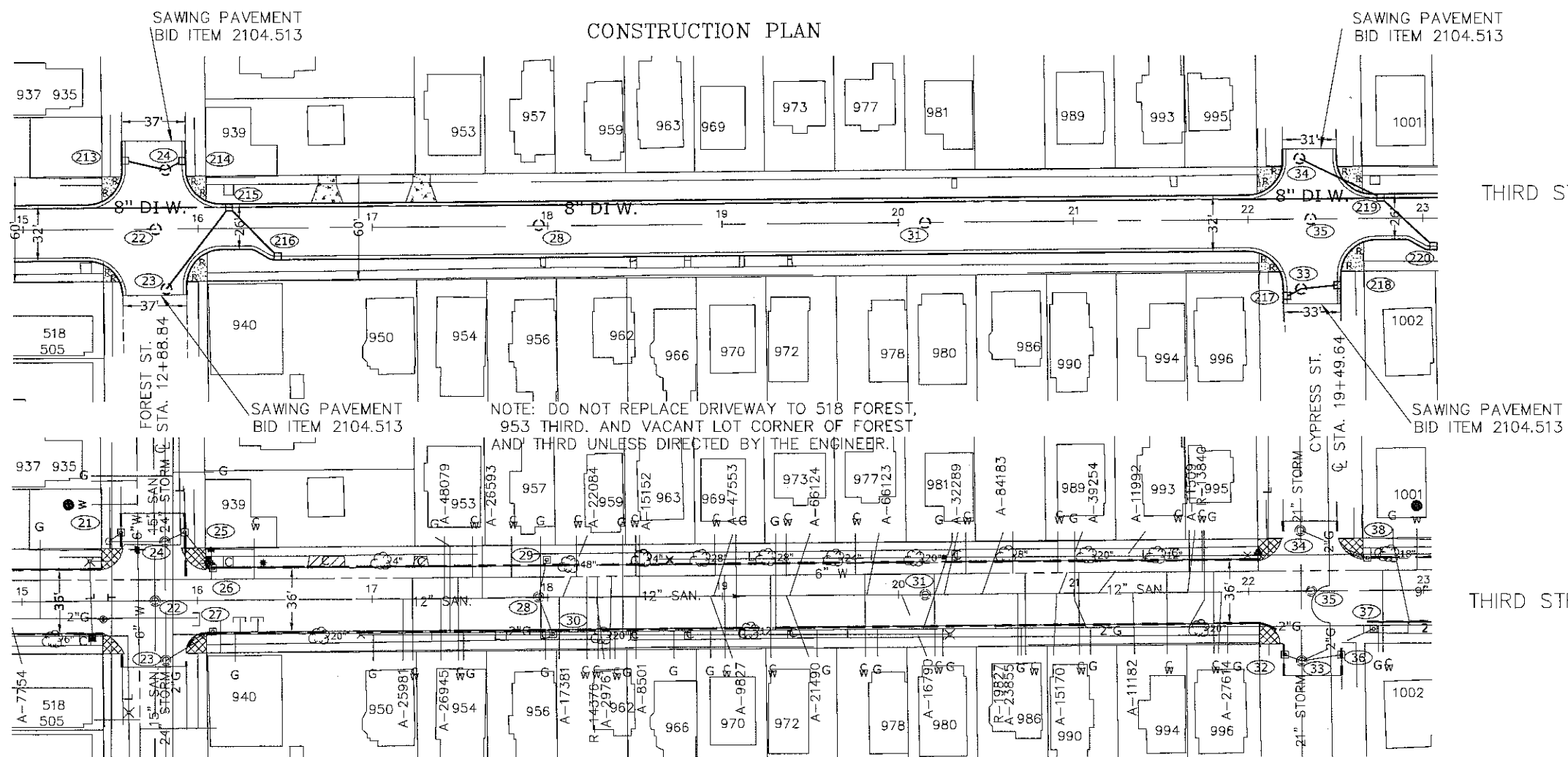
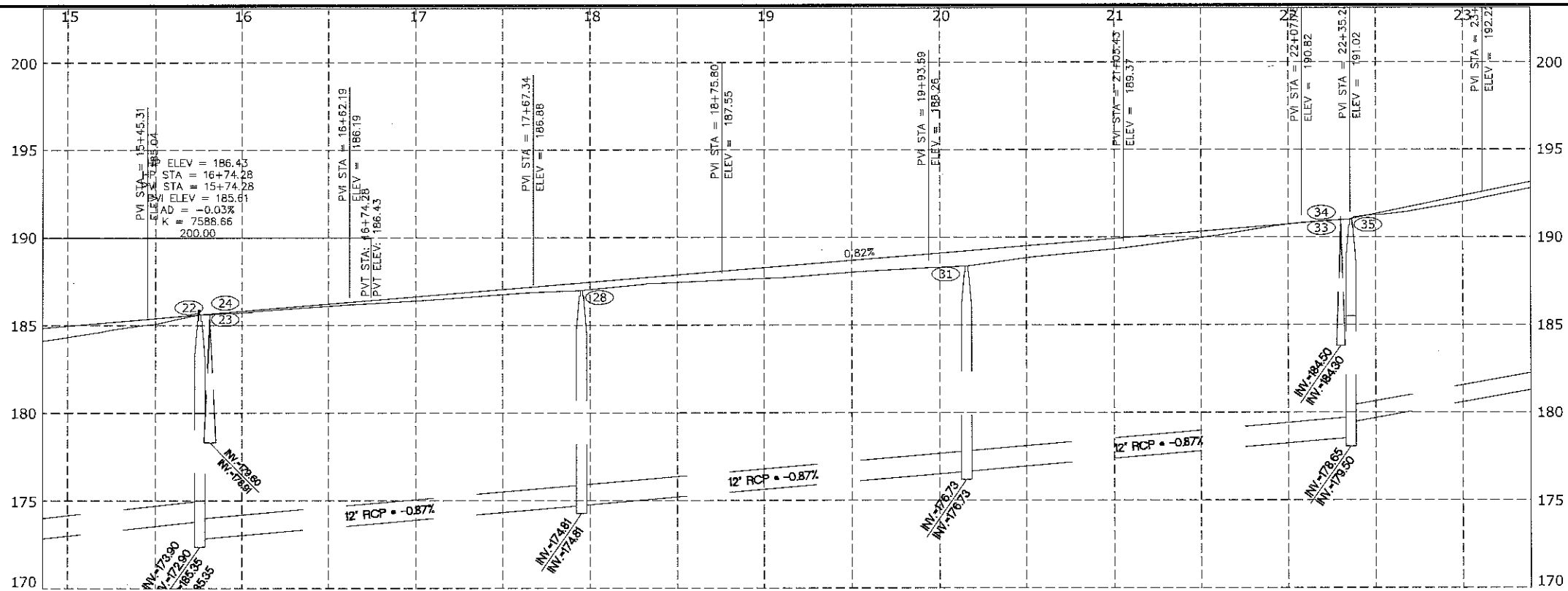
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	DESIGNED	BRM	I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA	PREPARED BY STREET ENGINEERING DIVISION FOR THE CITY OF ST. PAUL, DEPARTMENT OF PUBLIC WORKS	PROJECT: 15-P-1404	STATE AID PROJECT NUMBER:
	DRAWN	ATT			DRAWER: 12	CAD NAME: PROJECTS/CURRENT/
	APPROVED	BRM			DWG. NO. 1581	DATE: 1/21/15 SHEET NO. 10 OF 15 SHEETS

Signature: \_\_\_\_\_ Date: 1/21/15 Lic. No. 430999

THIRD STREET



APPROX. xxxx' OF 8" DI WATER MAIN IN NEW LOCATION IN THIRD STREET FOREST ST. TO CYPRESS ST. PIPE INSTALLATION BY SAINT PAUL REGIONAL WATER SERVICES, EXCAVATION BY CONTRACTOR AS BID ITEM 2504.603 "WATER MAIN TRENCH"

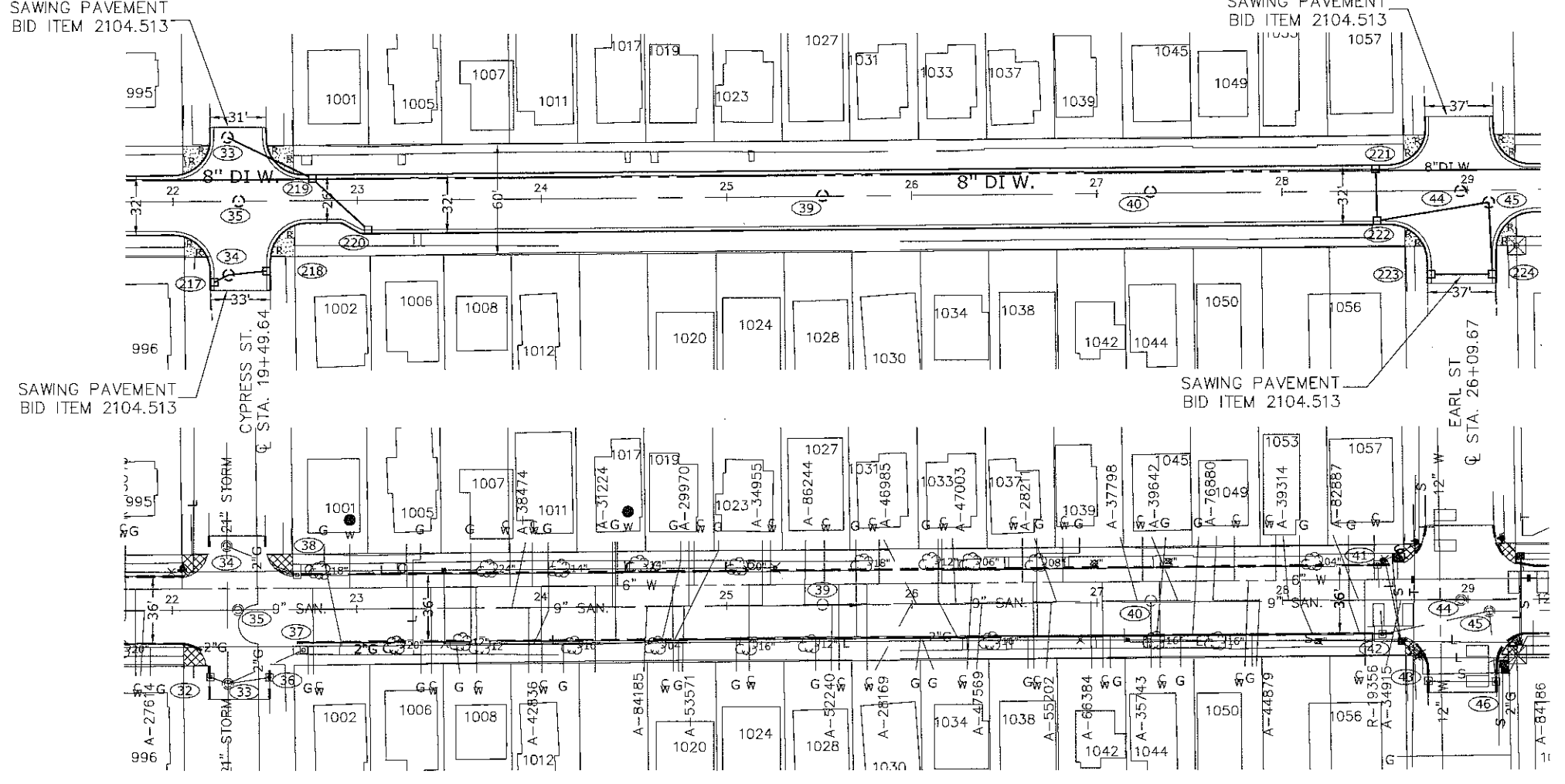
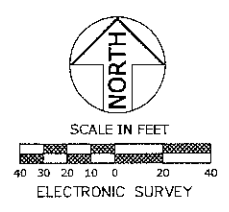
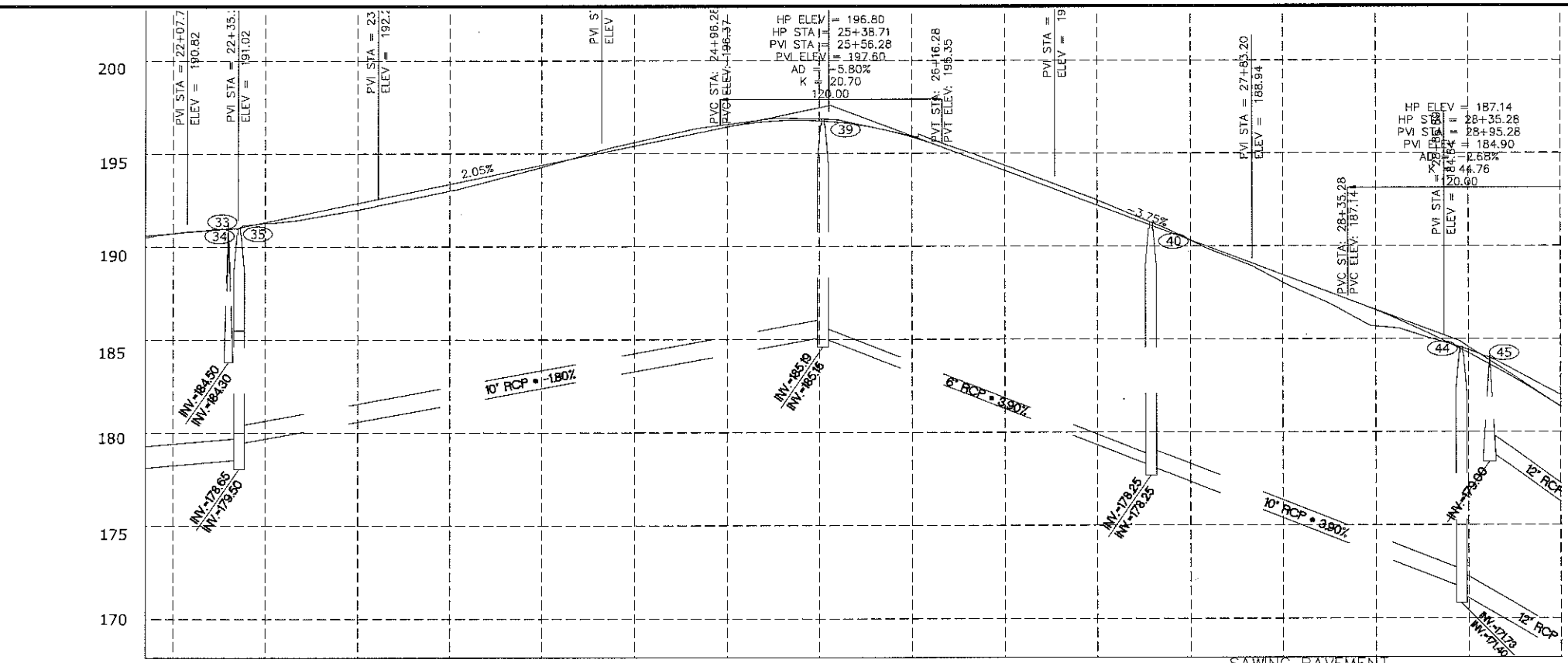
THIRD STREET  
Forest St. to Cypress St.

DESIGNED	BRM	I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA  Signature: _____ Date: 1/21/15 BARBARA R. MUNDAHL Lic. No. 43099
DRAWN	ATT	
APPROVED	BRM	

PREPARED BY STREET ENGINEERING DIVISION FOR THE CITY OF ST. PAUL, DEPARTMENT OF PUBLIC WORKS

## THIRD STREET

PROJECT: 15-P-1404	STATE AID PROJECT NUMBER:
DRAWER: 12	CAD NAME: PROJECTS/CURRENT/
DWG. NO. 1581	DATE: 1/21/15
SHEET NO. 11	OF 15 SHEETS



THIRD STREET

APPROX. xxxx' OF 8"DI WATER MAIN IN NEW LOCATION IN THIRD STREET CYPRESS ST. TO EARL ST. PIPE INSTALLATION BY SAINT PAUL REGIONAL WATER SERVICES, EXCAVATION BY CONTRACTOR AS BID ITEM 2504.603 "WATER MAIN TRENCH"

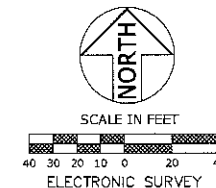
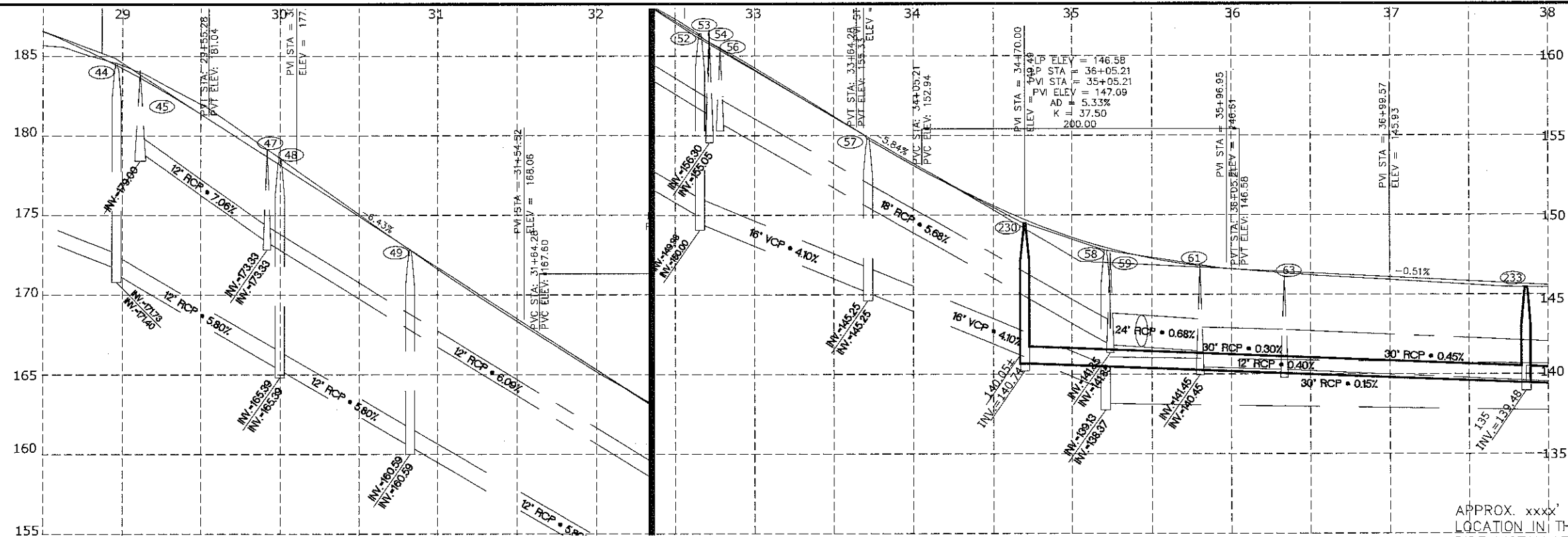
THIRD STREET

THIRD STREET  
Cypress St. to Earl St.

	DESIGNED	BRM	<p>I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA</p> <p>Signature: _____ Date: 1/21/15 Lic. No. 43099</p>	PREPARED BY STREET ENGINEERING DIVISION FOR THE CITY OF ST. PAUL, DEPARTMENT OF PUBLIC WORKS	PROJECT: 15-P-1404		STATE AID PROJECT NUMBER:	
	DRAWN	ATT			DRAWER: 12	CAD NAME: PROJECTS/CURRENT/		
	APPROVED	BRM			DWG. NO. 1581	DATE: 1/21/15 SHEET NO. 12 OF 15 SHEETS		

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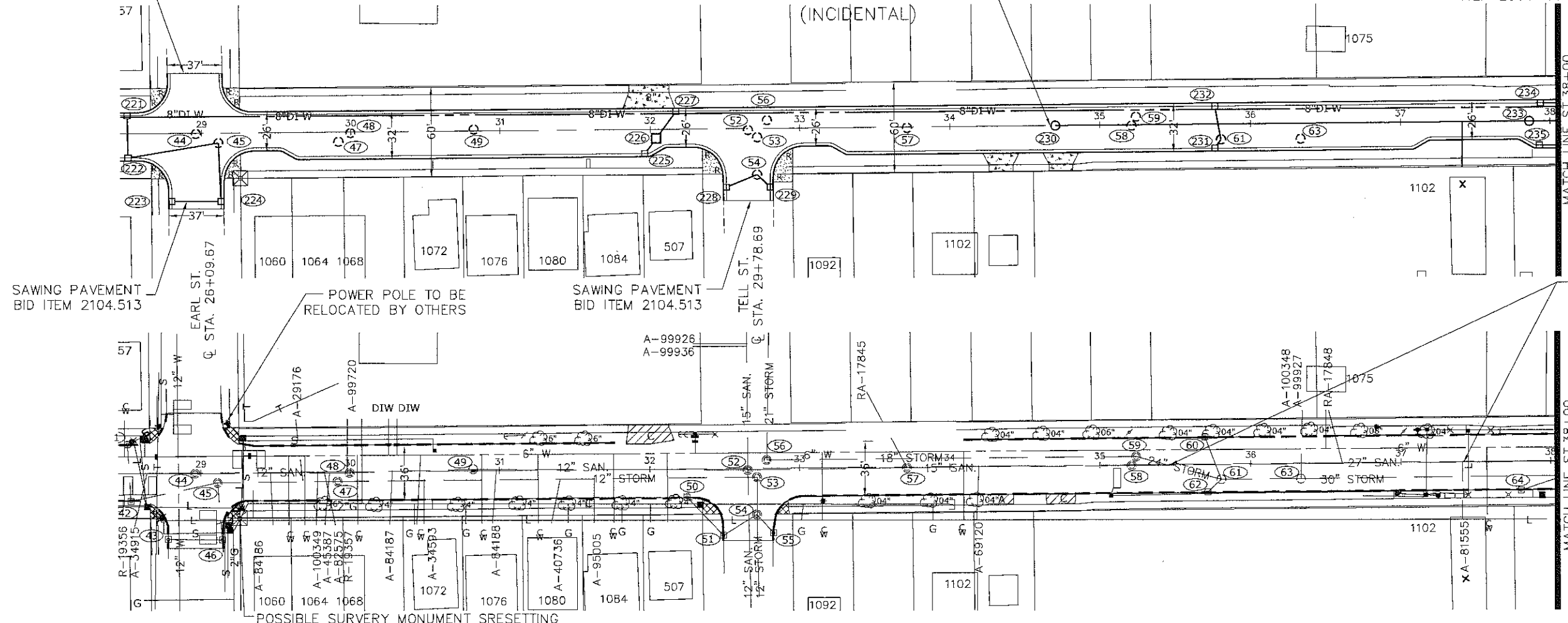


SAWING PAVEMENT  
BID ITEM 2104.513

CONNECT EXISTING 15"  
VCP TO MH 241  
(INCIDENTAL)

316 L.F. 12" PVC, C-900  
MH 242 TO MH 241

APPROX. xxxx' OF 8"DI WATER MAIN IN NEW  
LOCATION IN THIRD STREET EARL ST. TO ST.35+00.  
PIPE INSTALLATION BY SAINT PAUL REGIONAL WATER  
SERVICES, EXCAVATION BY CONTRACTOR AS BID  
ITEM 2504.603 "WATER MAIN TRENCH"



THIRD STREET

EXCAVATE STORM SEWER  
CROSSING SANITARY MAIN TO  
DETERMINE STORM SEWER INVERT  
AND EXCAVATE SEWER SERVICE  
CONNECTION AT MAIN TO  
DETERMINE LOCATION AND  
ELEVATION PRIOR TO MAINLINE  
SEWER REPLACEMENT.  
MAINLINE SEWER GRADE MAY  
BE ADJUSTED ONLY WITH THE  
APPROVAL OF THE ENGINEER.  
(PAYMENT AS PER BID ITEM  
2503.602 'SANITARY SEWER  
SERVICE REPAIR EXCAVATION 14'  
OR LESS IN DEPTH AT MAIN)

THIRD STREET

THIRD STREET  
Earl St. to Sta.38+00

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR  
UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED  
PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA

Signature: \_\_\_\_\_  
BARBARA R. MUNDAHL

Date: 1/21/15  
Lic. No. 43099

PREPARED BY STREET ENGINEERING DIVISION FOR THE CITY OF ST. PAUL, DEPARTMENT OF PUBLIC WORKS

THIRD STREET

PROJECT: 15-P-1404

DRAWER: 12

DWG. NO. 1581

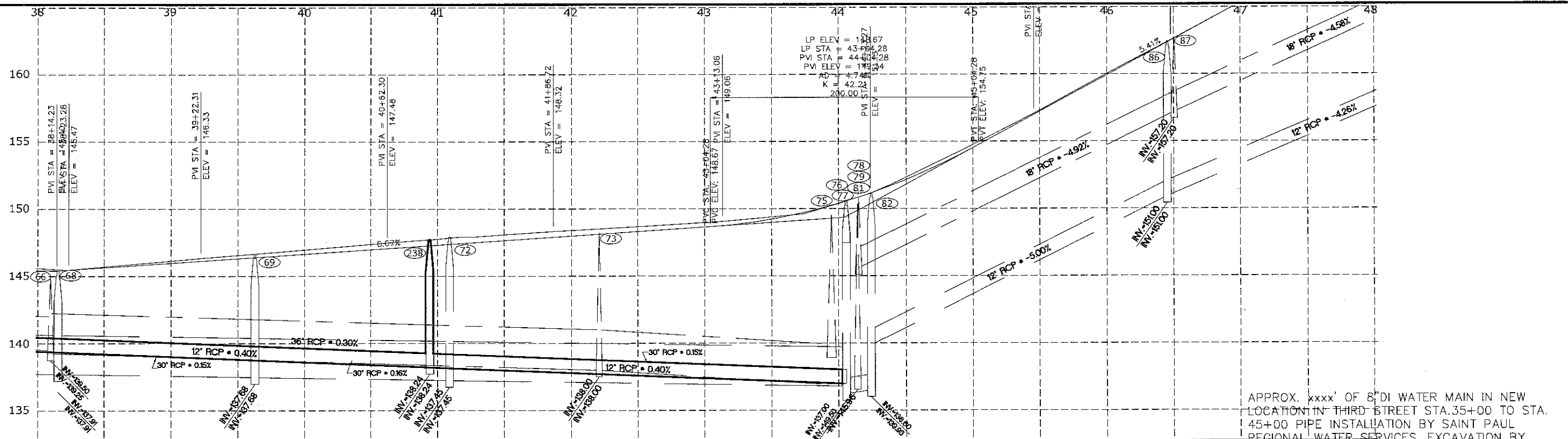
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PROJECT NUMBER:

CAD NAME: PROJECTS/CURRENT/

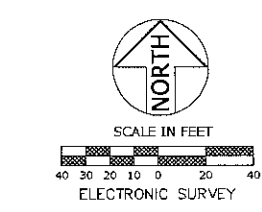
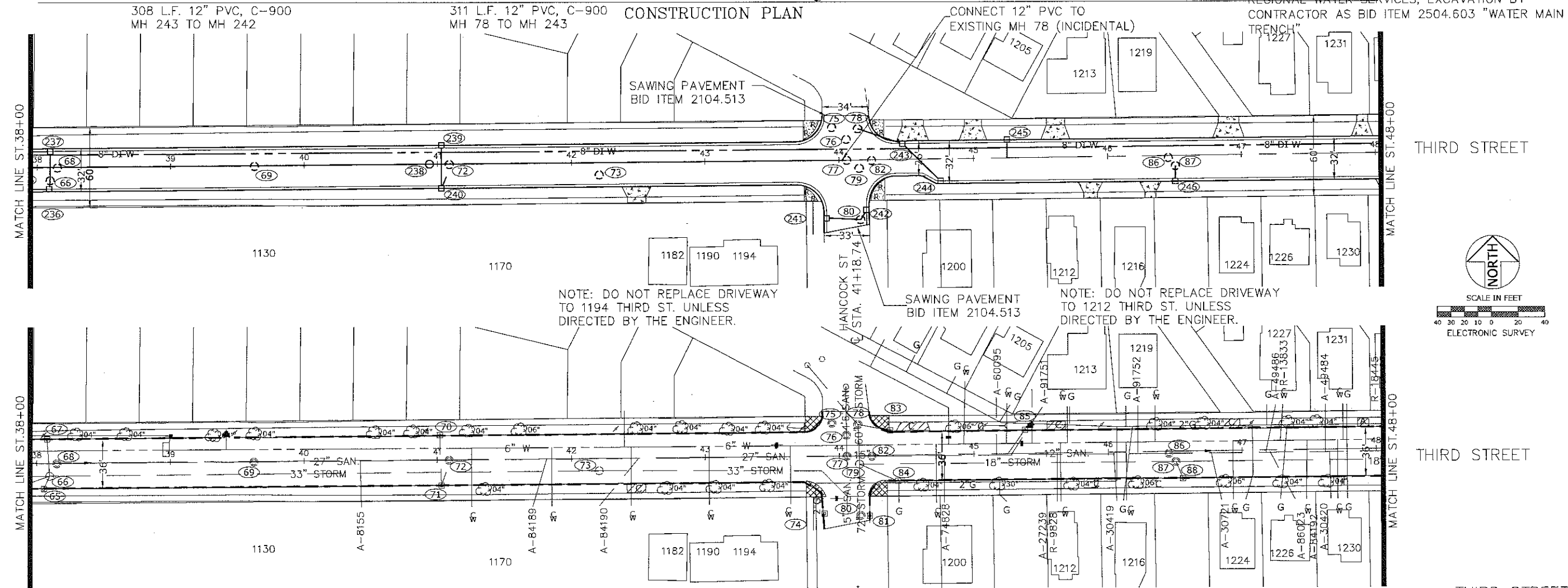
DATE: 1/21/15

SHEET NO. 13 OF 15 SHEETS





APPROX. xxxx' OF 8" DI WATER MAIN IN NEW LOCATION IN THIRD STREET STA.35+00 TO STA. 45+00 PIPE INSTALLATION BY SAINT PAUL REGIONAL WATER SERVICES, EXCAVATION BY CONTRACTOR AS BID ITEM 2504.603 "WATER MAIN TRENCH"

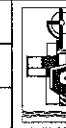


# EXISTING AND REMOVAL PLAN

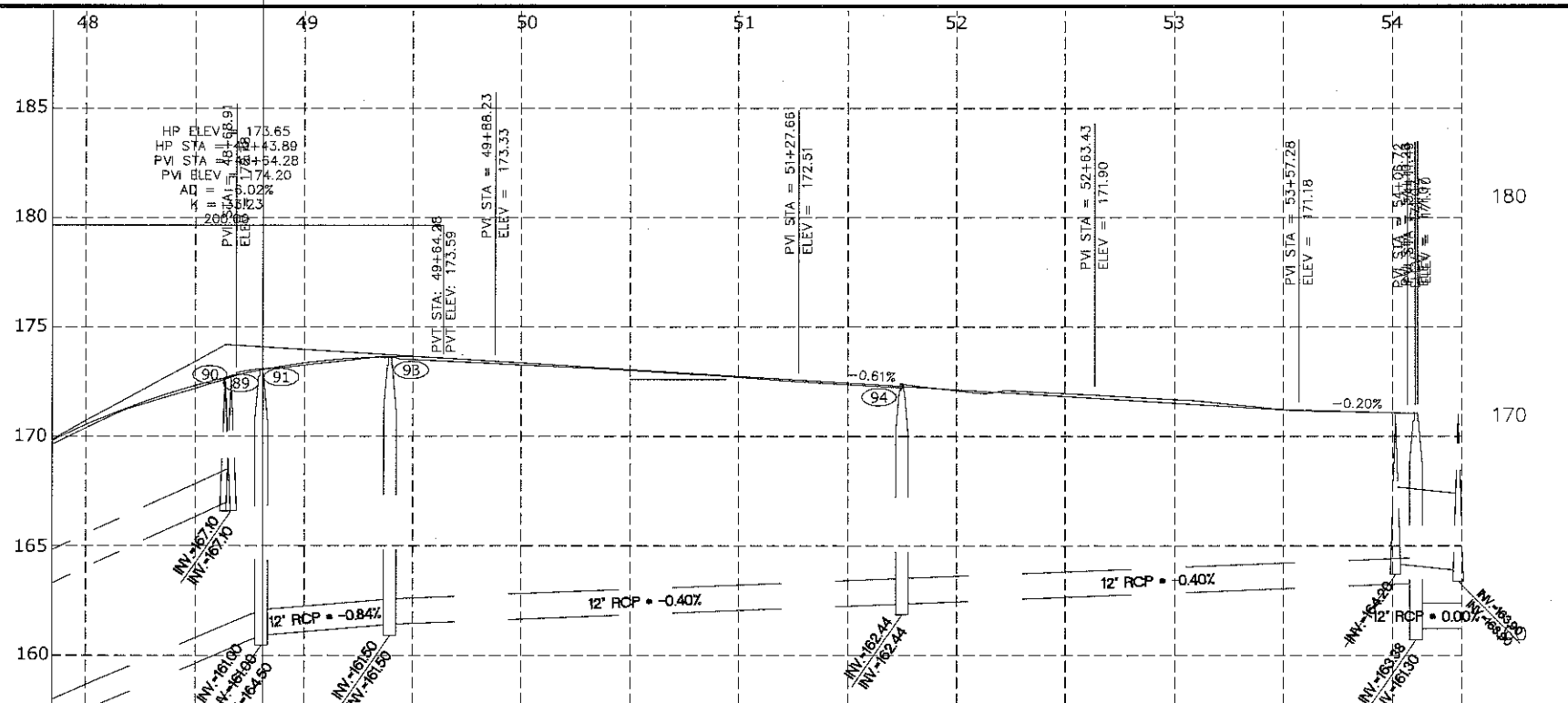
THIRD STREET  
Sta.35+00 to Sta.48+00.00

	DESIGNED	BRM	<p>I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA</p> <p>Signature: _____ Date: 1/21/15 BARBARA R. MUNDAHL Lic. No. 43099</p>		<p>PREPARED BY STREET ENGINEERING DIVISION FOR THE CITY OF ST. PAUL, DEPARTMENT OF PUBLIC WORKS</p>		<p>PROJECT: 15-P-1404 DRAWER: 12 DWG. NO. 1581</p>		<p>STATE AID PROJECT NUMBER: CAD NAME: PROJECTS/CURRENT/ DATE: 1/21/15 SHEET NO. 14 OF 15 SHEETS</p>	
	DRAWN	ATT								
	APPROVED	BRM								

## THIRD STREET

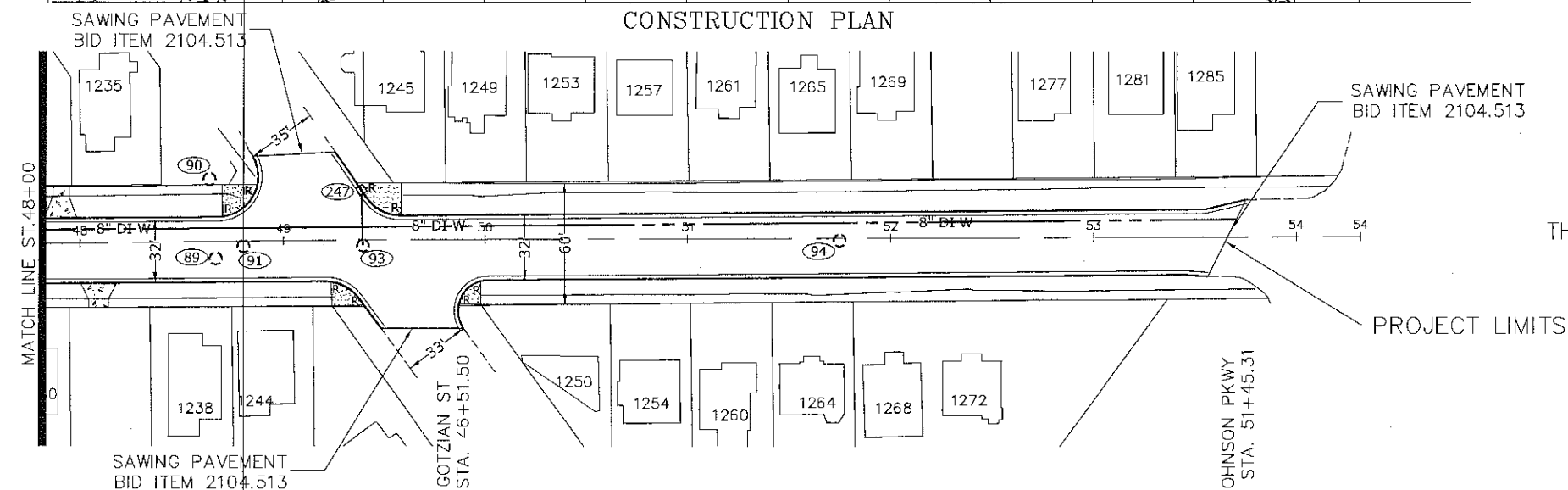


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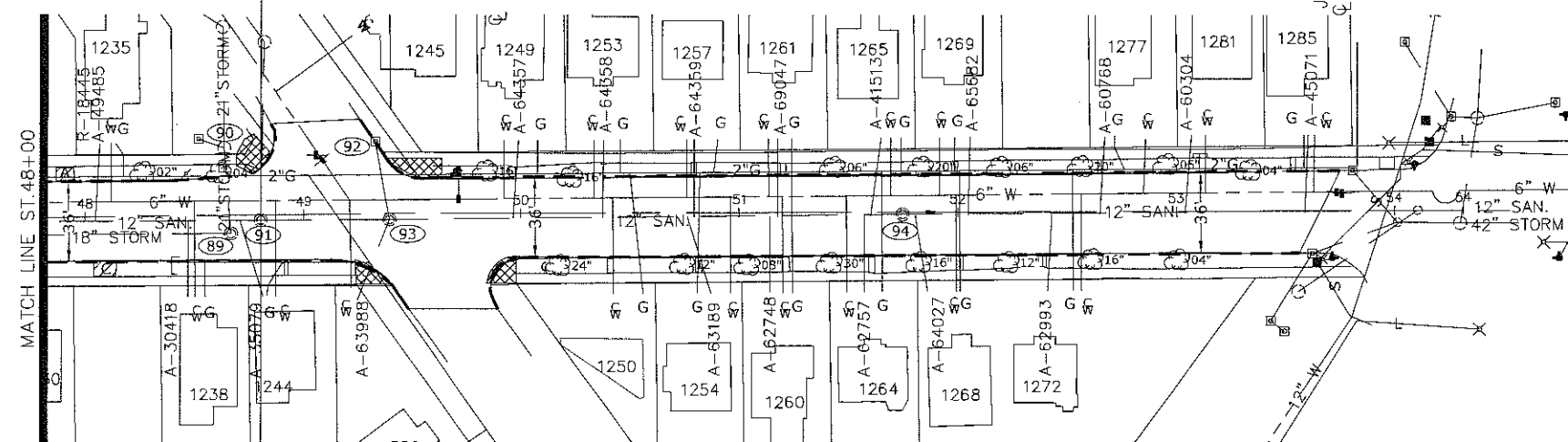
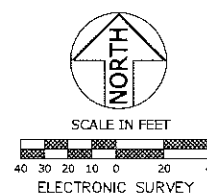
CONSTRUCTION PLAN

APPROX. xxxx' OF 8"DI WATER MAIN IN NEW LOCATION IN THIRD STREET STA. 45+00 TO JOHNSON PKWY. PIPE INSTALLATION BY SAINT PAUL REGIONAL WATER SERVICES, EXCAVATION BY CONTRACTOR AS BID ITEM 2504.603 "WATER MAIN TRENCH"



THIRD STREET

PROJECT LIMITS



THIRD STREET

EXISTING AND REMOVAL PLAN

THIRD STREET  
Sta.48+00.00 to Johnson Pkwy

DESIGNED	BRM	I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA  Signature: _____ Date: 1/21/15 BARBARA R. MUNDAHL Lic. No. 43099
DRAWN	ATT	
APPROVED	BRM	

PREPARED BY STREET ENGINEERING DIVISION FOR THE CITY OF ST. PAUL, DEPARTMENT OF PUBLIC WORKS

## THIRD STREET

PROJECT: 15-P-1404

DRAWER: 12

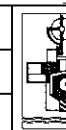
DWG. NO. 1581

STATE AID  
PROJECT NUMBER:

CAD NAME: PROJECTS/CURRENT/

DATE: 1/21/15

SHEET NO. 15 OF 15 SHEETS





## Transportation Committee Staff Report

*Committee date: 2/9/2015*

<b>Project Name</b>	<i>Saint Paul Bicycle Plan</i>
Geographic Scope	<i>Citywide</i>
Ward(s)	<i>All</i>
District Council(s)	<i>All</i>
Project Description	<i>Final draft of the Saint Paul Bicycle Plan. The plan designates corridors for future development of bikeway and addresses other policies and topics related to bicycling.</i>
Project Contact	<i>Reuben Collins</i>
Contact email/phone	<i>651-266-6059</i>
Lead Agency/Department	<i>Department of Public Works</i>
Purpose of Project/Plan	<i>The plan designates corridors for future development of bikeways and provides recommendations regarding bikeway facility types for those corridors. The plan will be used by Public Works as an implementation guide.</i>
Planning References	<i>Comprehensive Plan, Bike Walk Central Corridor Action Plan</i>
Project stage	<i>Planning</i>
General Timeline	<i>Previous drafts of the plan were released on 1/21/2014 and 10/6/2014. This final version of the plan is recommended for adoption.</i>
District Council position (if applicable)	<i>None</i>
Level of Committee Involvement	<i>Involve</i>
Previous Committee action	<i>Presentations regarding this plan were presented to the Transportation Committee on 6/13/2011, 4/22/2013, 1/27/2014, 5/19/2014, 10/6/2014, and 1/12/2015.</i>
Level of Public Involvement	<i>Involve</i>
Public Hearing	<i>A public hearing at the Planning Commission was held on 12/5/2014.</i>
Public Hearing Location	<i>Planning Commission</i>
Primary Funding Source(s)	<i>Unknown</i>
Cost	<i>Unknown</i>

Staff recommendation	<i>Recommend approval of the Saint Paul Bicycle Plan. Review &amp; modify recommended language for a Planning Commission Resolution.</i>
Action item requested of the Committee	<i>Vote to recommend approval of the Saint Paul Bicycle Plan to the Planning Commission. Vote to recommend language for a Planning Commission resolution.</i>
Cmmttee recommendation	<i>N/A</i>
Committee vote	<i>N/A</i>



**CITY OF SAINT PAUL**  
*Christopher B. Coleman, Mayor*

*1500 City Hall Annex  
25 W. Fourth Street  
Saint Paul, MN 55102-1660*

*Fax: 651-266-6222*

TO: Transportation Committee

FROM: Reuben Collins, Department of Public Works

DATE: 2/9/2015

**SUBJECT: Summary of Public Hearing Feedback and Recommendations on draft Saint Paul Bicycle Plan**

On December 5, 2014, the Planning Commission held a public hearing regarding the draft Saint Paul Bicycle Plan (SPBP). Since the most recent draft of the SPBP was released on October 6, 2014, the following statements have been received by city staff.

- 33 statements were delivered at the public hearing.
- 98 statements were received through the Open Saint Paul online tool.
- 42 statements were received via email
- 6 Statements were received from other groups or organizations: CapitolRiver Council, District 1 Community Council, Fresh Energy, Friends of the Parks and Trails, Lower Phalen Creek Project, Sierra Club, Smart Trips/Women on Bikes
- Saint Paul Area Chamber of Commerce, Saint Paul Building Owners and Managers Association, & Wabasha Partners reiterated their previous statement dated April 1, 2014.

In some cases, individuals submitted similar or identical statements through multiple channels.

This feedback received is in addition to the feedback received on the first draft of the SPBP between January and April, 2014, when staff received nearly 400 written statements, including statements from 10 District Councils. This is also in addition to the feedback received from the public through several phases of public involvement and outreach since 2011 when the planning process began. Appendix A, B, C, and E of the current draft of the SPBP summarize all of the feedback received since 2011.

The statements received were evaluated and subjectively placed into one of the following four categories:



*An Affirmative Action Equal Opportunity Employer*



Statement Characterization	% of Statements Received
Support the SPBP as is. No recommendations or concerns were stated.	40%
Support the SPBP, but offered recommendations for improvement or expressed a concern.	35%
Did not specifically state support or opposition to the SPBP, but offered recommendations or expressed a concern.	10%
Opposed to the SPBP.	15%

The testimony received was generally supportive of the SPBP. Supporters cited reasons such as quality of life, economic development potential, safety, livability, a desire for transportation options, affordability, and health benefits. Those opposed to the SPBP cited reasons such as cost, misguided priorities, and impacts to parking.

## Recommended Changes in Response to Statements

The following is a list of the most frequent concerns or recommendations raised at the public hearing or received through other channels, along with staff recommendation or response.

- **Parking** – The largest concern voiced at the public hearing was concern regarding potential loss of parking in downtown due to construction of the downtown loop & spur trail concept proposed in the SPBP. Commenters concerns ranged from a general lack of parking in the downtown area, the need for more convenient parking near retail storefronts, the importance of on-street parking on particular streets such as Wabasha Street, and concerns about the price of parking (too expensive).
  - **Staff Response:** Staff recommends no changes to the SPBP. Staff is currently conducting a downtown parking study to help understand issues relating to parking and develop strategies for improving the overall parking situation in downtown. This study will be complete in early 2015. The SPBP recommends that further study is needed of the downtown loop & spur before implementation to allow time for additional conversation about parking and other issues. That additional study of the downtown loop & spur would also happen in 2015 to allow the two studies to inform each other.
- **Bicycle Parking Ordinance:** Numerous individuals voiced concern with Action Item 7.1.1, which recommended that the City consider adopting an ordinance that would prohibit locking bicycles to certain objects in the public right-of-way. Many commenters felt that there is not currently enough bicycle parking, thus resulting in bicycles locked to things

other than bike racks. Many felt it was inappropriate to begin restricting bicycle parking options while there exists a deficit of legitimate bike parking options in many locations.

- **Staff Recommendation:** Staff recommends removing Action Item 7.1.1, and reiterates a commitment to Action Items 7.1.4 and 7.1.5 (to be renumbered 7.1.3 and 7.1.4), which address concerns regarding a lack of bicycle parking. While the action item will be removed, the SPBP text will retain general statements that it is undesirable for bicycles to be locked to certain objects within the public right-of-way.
- **Recommending bikeways on Arterials vs. parallel routes** - Several statements questioned the purpose of recommending bikeways be developed along arterials, instead recommending that bikeways be identified on streets with lower motorized traffic volumes. Specific questions were raised regarding Cleveland Avenue and Fairview Avenue. However, these comments are balanced by comments from other individuals stating that not enough of the arterials are represented for bikeways in the SPBP, specifically with reference to West 7th and East 7th. Smart Trips included a recommendation in their statement that “the addition of more arterials as bicycle routes should be considered”, citing the directness of routes and access to destinations along arterials.
  - **Staff Recommendation:** Staff recommends no changes to the SPBP. Discussion about whether bicyclists and the city as a whole are best served by having bicycle routes on arterials or parallel routes has been a common theme throughout the creation of the SPBP and is a topic of much discussion nationally and in every city that endeavors to craft a bicycle plan. The preferences of bicyclists are diverse, and the SPBP strives to strike a balance between bicycle routes on arterials and routes on parallel routes.
- **Maintenance** – Several comments recommended that the SPBP address winter maintenance procedures. Statements recommended setting minimum maintenance standards or identifying maintenance schedules or procedures.
  - **Staff Recommendation:** Staff recommends no changes to the SPBP. Staff agrees this is an important topic, but developing maintenance standards is outside the scope of this planning effort. However, maintenance issues can be addressed in future studies or planning efforts.
- **Update Timeline** – Several commenters stated that the recommendation for the SPBP to be updated in 5-7 years was too long a timeframe and that more frequent updating would be desirable.
  - **Staff Recommendation:** Staff recommends no changes to the SPBP. Completion of the Comprehensive Plan in 2018 will provide an interim opportunity for assessing progress. An update to the SPBP may be initiated sooner than 5-7 years if it becomes clear that an update is needed.
- **Citywide traffic speeds and Truck Routes** – Several commenters mentioned a desire to consider lowering speed limits throughout the city, or on residential streets, citing safety concerns. Others commented regarding prohibiting truck traffic on certain routes, particularly along Raymond Avenue.
  - **Staff Recommendation:** Staff recommends no changes to the SPBP. Citywide traffic speeds and modifications to freight routes are outside the scope of this planning effort.

- **Education, Enforcement, & Encouragement** – Several commenters requested that the SPBP address issues of education, encouragement, and enforcement of traffic laws.
  - **Staff Recommendation:** Staff recommends the addition of Section 8.7: Education, Encouragement, & Enforcement, to mention the importance of these topics and to discuss how the City might seek partnerships with other agencies such as MnDOT or other advocacy groups to create materials, programs, or initiatives regarding education, encouragement, & enforcement. The text includes an action item to pursue these efforts.
- **Cost** – Several commenters expressed concern about cost. Some felt that any investment in bicycle infrastructure is unwarranted and should not be a priority. Others expressed concern about the cost of certain recommendations, the downtown loop & spur in particular. Others were not concerned about the cost, but thought that the planning level cost estimates presented in Section 9.6 overestimated the costs of developing bikeways.
  - **Staff Recommendation:** Staff recommends adding additional text to Section 9.6 clarifying that bundling bicycle projects with other roadway projects is anticipated to bring significant cost savings.
- **Case Avenue, Jessamine Avenue, and Lawson Avenue** – Several commenters mentioned concerns about changes to the east/west routes proposed for the Payne-Phalen neighborhood. The first draft of the SPBP had recommended routes on Case Avenue and Jessamine Avenue. In the second draft of the SPBP, these two routes were removed and replaced by a single planned route on Lawson Avenue.
  - **Staff Recommendation:** Staff recommends returning to the original routes proposed in the first draft of the SPBP, which includes routes proposed for Case Avenue and Jessamine Avenue.
- **Prioritization** – Several commenters requested additional clarification regarding prioritization strategies. The draft SPBP had identified 15 prioritization principles to be used in helping to identify priorities, however, many felt that they weren't helpful, or that it wasn't clear how they would be used to identify potential projects.
  - **Staff Recommendation:** Staff recommends revising the 15 prioritization principles down to a less cumbersome 5 prioritization principles. A methodology for creating a prioritization matrix is proposed to be used to aid in making decisions about how to prioritize elements of the bicycle network.

## Other Recommended Changes

City staff also proposed the following changes to the document as a result of additional internal dialogue between departments and additional conversation with Metropolitan Council staff regarding consistency with the draft 2040 Transportation Policy Plan (TPP) and 2040 Regional Parks Policy Plan (RPPP), which are anticipated to be adopted in early 2015.

- **Section 4.0 Policy and Planning Context** – The first draft of the SPBP released in January 2014 included an extensive discussion of the roles and responsibilities of partner agencies such as the Metropolitan Council, MnDOT, Ramsey County, & the DNR. The second draft of the SPBP released on October 2014 moved much of this text to the appendix in an effort to improve readability and clarity. However, after discussion with Metropolitan Council staff,

city staff recommends moving much of the text back into Section 4.0 and eliminating the appendix item. This section will include additional discussion of the Regional Bicycle Transportation Network (RBTN) that was not in previous drafts.

- **Section 5.2 – Bicycle Network Functional Classification** – The SPBP included language about the Metropolitan Council being “in process” of updating the TPP. Staff recommends removing outdated language relating to the RBTN in light of anticipated adoption of the TPP before adoption of the SPBP. This section will also include stronger language about the challenges associated with the use of freight railroad corridors for bikeway development.
- **Section 6** – Staff recommends inserting a new section 6.4 to discuss the RBTN. A new figure will be inserted (as Figure 6) to present the RBTN in Saint Paul. The text will include an action item to identify specific RBTN alignments within the search corridors.
- **Section 6.5 –Regional Trail Improvements – (renumbered from Section 6.4)** – Staff recommends revising the language to be more consistent with language used in the 2040 Regional Parks Policy Plan, due to be adopted by the Metropolitan Council in advance of the SPBP. The associated Figure 7 (renumbered from Figure 6) relating to Regional Trail has been significantly revised, though the inherent recommendations remain the same. Usage of terms such as “regional trail search corridor” or “planned regional trail” have been revised to be more consistent with the 2040 RPPP.
- **Section 7.2 Showers, Lockers, and other Amenities** – The SPBP suggested considering a change to the zoning code to encourage or require showers and locker rooms in some developments. After additional discussion with zoning and planning staff, it is unclear if the zoning code is the appropriate tool to accomplish the intent of this action item. Staff recommends that the text be amended to allow for the zoning code or other planning and regulation tools to accomplish the intent.
- **Trout Brook Regional Trail** – A recommendation for an off-street path along Jackson Street from Maryland to Arlington has been added as a possible interim alignment for the Trout Brook Regional Trail until the freight railroad spur becomes available for trail use.
- **Minor Route Adjustments** – Minor adjustments have been made to routes on Saratoga Street and Pascal Street to suggest a connection will be made using the planned CP Rail Trail. Minor Adjustments have been made to routes on Fuller Avenue, Aurora Avenue, and Shields Avenue to be more consistent with the recommendations in the Bike/Walk Central Corridor Action Plan. A small trail connection has been added within Victoria Park.
- **General Formatting and Appearance** –A new cover sheet has been added and pagination has been adjusted. Table numbers have been added, and the contents of the tables have been updated to reflect additions to the bicycle network that have taken place since the draft was released.

## Recommendation

Staff recommends that the Saint Paul Bicycle Plan, as amended, be recommended for consideration by the Planning Commission to be adopted as an addendum to the Comprehensive Plan.

city of saint paul  
planning commission resolution  
file number \_\_\_\_\_  
date \_\_\_\_\_

### **Saint Paul Bicycle Plan**

WHEREAS, the City of Saint Paul Comprehensive Plan includes a number of strategies for improving the bicycle network and increasing bicycling in Saint Paul, such as:

Transportation 3.4 Develop and maintain a complete and connected bikeway system.

T3.4 Develop and maintain a complete and connected bikeway system.

T3.5 Support existing off-street shared-use paths and add facilities and amenities supportive of active living principles.

T3.6 Fill gaps in the bikeway system.

T3.8 Promote “bicycle boulevards as a new type of bikeway; and

WHEREAS, staff from the Departments of Public Works, Planning and Economic Development, and Parks and Recreation, contributed to a draft “Saint Paul Bicycle Plan”; and

WHEREAS, from 2011 to 2013, city staff solicited input to help in the drafting of the “Saint Paul Bicycle Plan” through holding open houses, web-based surveys, and through Open Saint Paul; and

WHEREAS, in January 2014, a first draft of the “Saint Paul Bicycle Plan” was released; and

WHEREAS, between January and April, 2014, a series of open houses were held and presentations made to various District Councils and other interested community groups; and

WHEREAS, comments from the open houses and other community meetings were overwhelmingly positive regarding the “Saint Paul Bicycle Plan,” with specific requests to enlarge the scope of the Plan, change certain bikeway alignments in the Plan, and clarify certain components in the Plan; and

WHEREAS, the Transportation Committee considered the Plan and the comments received on 5/19/2014 and provided recommendations for revisions to the Plan; and

WHEREAS, revisions were made to the draft “Saint Paul Bicycle Plan” to reflect and address the input of the community and the Transportation Committee; and

WHEREAS, a second draft of the “Saint Paul Bicycle Plan” was released in October 2014; and

WHEREAS, the Planning Commission, on 10/17/2014, released the draft of the “Saint Paul Bicycle Plan” for formal public review, and set a public hearing for 12/5/2014; and

WHEREAS, a public hearing notice was published pursuant to Minnesota Statute § 462.357, Subd. 5, and sent to the early notification list and other interested parties; and

WHEREAS, a public hearing on the draft “Saint Paul Bicycle Plan” was conducted by the Planning Commission on 12/5/2014, at which all persons present were allowed to testify; and

WHEREAS, the written record was left open until 4:30 PM on 12/8/2014; and

WHEREAS, the public hearing comments generally supported the draft “Saint Paul Bicycle Plan” with specific comments on impacts to parking, bikeway maintenance, ordinance revisions regarding bicycle parking, and education and enforcement efforts; and

WHEREAS, the Planning Commission referred the draft “Saint Paul Bicycle Plan” to the Transportation Committee for consideration of the public testimony and possible revisions to the draft Plan; and

WHEREAS, the Transportation Committee discussed the comments, draft “Saint Paul Bicycle Plan”, and proposed changes on 1/12/2015 and 2/9/2015; advised on revisions to the Plan and Figures; and forwarded its recommendation to the Planning Commission; and

WHEREAS, the Planning Commission considered the public testimony and the recommendations of the Transportation Committee on the “Saint Paul Bicycle Plan”; and

WHEREAS, the “Saint Paul Bicycle Plan” is found to be consistent with the Saint Paul Comprehensive Plan and other addenda;

NOW, THEREFORE, BE IT RESOLVED, that the Planning Commission recommends that the City Council adopt the “Saint Paul Bicycle Plan” as an addendum to the “City of Saint Paul Comprehensive Plan.”